

**LAND SUBDIVISION REGULATIONS  
OF EUHARLEE, GEORGIA**

**Adopted September 2, 2003  
Amended September 20, 2006  
Amended September 5, 2023**

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# LAND SUBDIVISION REGULATIONS

## EUHARLEE, GEORGIA

### §1-1 SUBDIVISIONS AND LAND DEVELOPMENT

#### §1-1-1 TITLE

These regulations shall be known and may be referred to as the Land Subdivision Regulations of Euharlee, Georgia.

#### §1-1-2 PURPOSES

This Ordinance is adopted for the following purposes.

- (a) Promote the orderly, planned, efficient, and economic development of the City and to guide future growth in accordance with the Comprehensive Plan.
- (b) Ensure that lands subdivided are of such character that they can be used for building purposes without danger to the health or safety of residents, and to secure safety from fire, flood, or other menace.
- (c) Prevent the pollution of air, land, streams, and ponds, as well as encourage the wise use and management of natural resources throughout the City, and preserve the topography and beauty of the community and the value of land.
- (d) Ensure the proper provision of improvements such as drainage, water, sewerage, and capital improvements such as schools, parks, playgrounds, recreational facilities, and transportation facilities.
- (e) Provide for open spaces through the most efficient design and layout of the land.
- (f) Establish procedures for the subdivision and re-subdivision of land in order to further the orderly development of land.
- (g) Provide for the proper monumenting of subdivided land and proper legal descriptions.
- (h) Help eliminate the costly maintenance problems that develop when streets and lots are established without proper consideration given to various public purposes.
- (i) Facilitate and inform lot purchasers who generally lack the specialized knowledge needed to evaluate subdivision improvements and design.

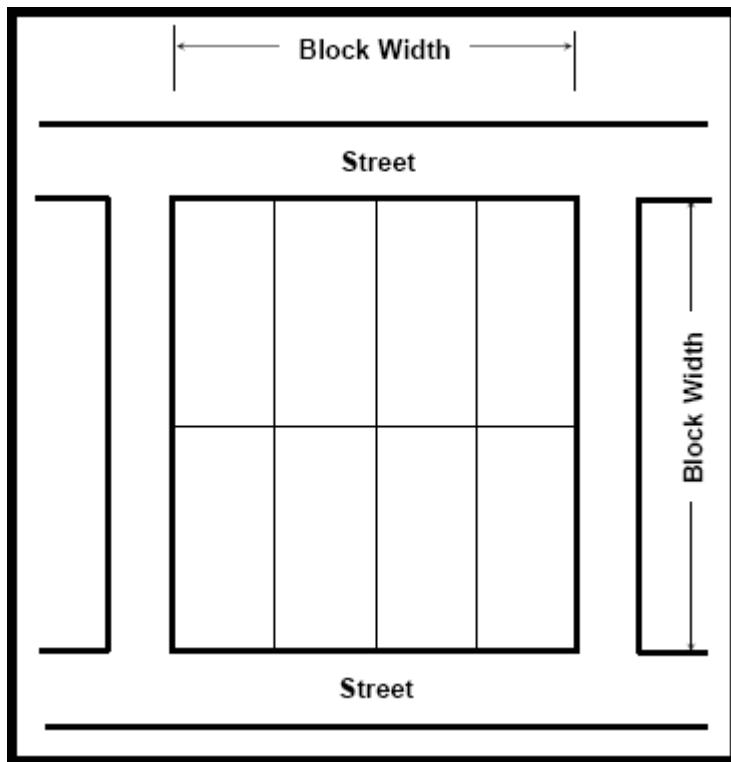
#### §1-1-3 DEFINITIONS

Alley: A strip of land dedicated to public use providing vehicular and pedestrian access to the rear of properties which abut and are served by a public road or street.

Aggrieved: As it relates to an aggrieved party's right to appeal, "aggrieved" shall mean being directly and negatively impacted by a decision, such as an adjoining landowner, an applicant who is denied, or person in similar situation. It shall also mean any City Council official or the Mayor if they disagree with a decision of City staff. *Amended September 20, 2006.*

**Block:** An area of land within a subdivision that is entirely surrounded by public streets, public lands, railroad rights-of-way, watercourses, or other well defined and fixed boundaries. (See Figure 1-1-3.1).

**Figure 1-1-3.1**  
Block, Block Length and Block Width



**Comprehensive plan:** Any plan adopted by the Mayor and City Council of the City of Euharlee, or portion of such plan or plans. This definition shall be construed liberally to include the major thoroughfare plan, master parks and recreation plan, or any other study, document, or written recommendation pertaining to subjects normally within the subject matter of a Comprehensive Plan as provided by the Georgia Planning Act of 1989, if formally adopted by the Mayor and City Council.

**Conservation areas, primary:** Any property qualifying as conservation use property under O.C.G.A. Section 48-5-7.4; and any steep mountain slopes, floodplains, wetlands, waterbodies, upland buffers around wetlands and water bodies, critical wildlife habitat, and sites of historic, cultural, or archaeological significance, located outside of building envelopes and lots established for building purposes.

Conservation areas, secondary: Prime farmland, natural meadows, mature woodlands, farm fields, localized aquifer recharge areas, and lands containing scenic views and sites, located outside of building envelopes and lots established for building purposes.

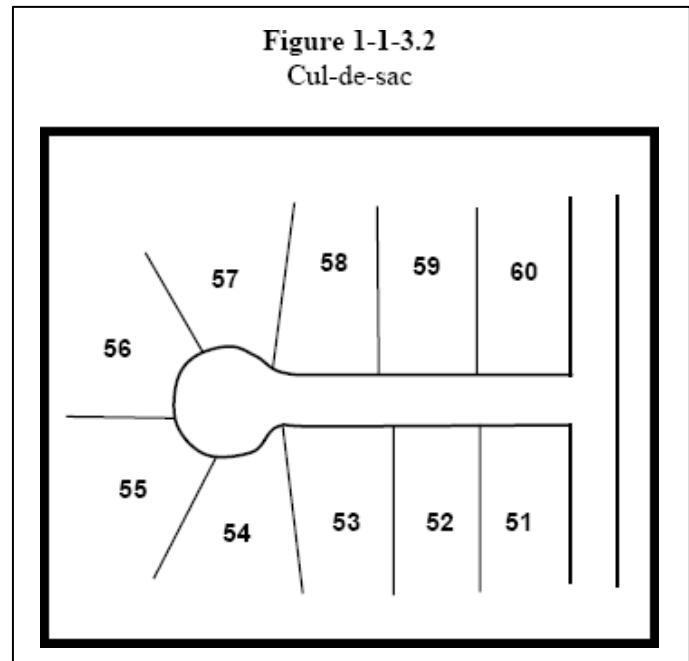
Conservation easement: A legally enforceable agreement between a property owner and the holder of the easement, in a form acceptable to the City Attorney and recorded in the office of the Clerk of Superior Court of Bartow County. A conservation easement restricts the existing and future use of the defined tract or lot to conservation use, agriculture, passive recreation, or other use approved by the Mayor and City Council and prohibits further subdivision or development. Such agreement also provides for the maintenance of open spaces and any improvements on the tract or lot. Such agreement cannot be altered except with the express written permission of the easement holder and any other co-signers. A conservation easement may also establish other provisions and contain standards that safeguard the tract's or lot's special resources from negative changes.

Conservation subdivision: A subdivision, as defined by this Ordinance, where open space is the central organizing element of the subdivision design and that identifies and permanently protects all primary and all or some of the secondary conservation areas within the boundaries of the subdivision.

Contiguous common parcels: Parcels adjoining or touching other land at a common point and having a common owner, regardless of whether or not portions of the parcels have separate tax lot numbers, or were purchased in different land lots, or were purchased at different times.

Cul-de-sac: A dead-end street of limited length having a primary function of serving adjoining land, and constructed with a turnaround at its end (see Figure 1-1-3.2).

Cul-de-sac, temporary: A nonpermanent vehicular turn around located at the termination of a street or alley.



Deceleration lane: An added roadway lane, of a specified distance and which may include a taper, as approved by the City Engineer that permits vehicles to slow down and leave the main vehicle stream.

Dedication: The deliberate appropriation of land by an owner for any general and public use or purpose, reserving to himself no other rights than such as are compatible with the full exercise and enjoyment of the public uses to which the property has been devoted.

Dedication plat: A plat that indicates property to be dedicated for public right-of-way or land for public use.

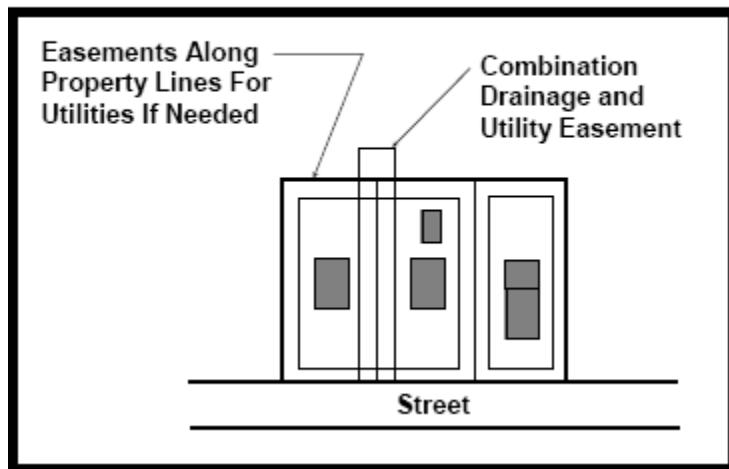
Development: Any manmade change on improved or unimproved real estate, including but not limited to, buildings, structures, mining, dredging, filling, grading, paving, excavation, drilling, or permanent storage of materials or equipment.

Director: The Zoning Administrator, or his designee.

Easement: A grant of one or more of the property rights by the property owner to and/or for use by the public, a corporation, or another person or entity. (See Figure 1-1-3.3).

**Figure 1-1-3.3**

Easements



Escrow account: A type of subdivision improvement guarantee where the subdivider deposits either cash, a note, a bond, or some other instrument readily convertible to cash for specific face value specified by the City to cover the costs of required improvements.

Final plat: The final drawing of a subdivision and, as applicable, dedication, prepared for filing for record with the Clerk of the Bartow County Superior Court, and containing all elements and requirements set forth in this Ordinance.

Habitat for endangered or threatened species: An area verified by the Georgia Department of Natural Resources as; 1) actually containing naturally-occurring individuals of a species that has been listed as endangered or threatened under the Federal Endangered Species Act, as amended, and, 2) being likely to support the continued existence of that species by providing for a significant portion of that species' biological requirements, and that meets the definition of "natural conditions" as defined by this Ordinance.

Half street: A portion of the ultimate width of a road or street where the remaining portion of the road or street shall be provided at a future date.

Home owners association: An organization formed for the maintenance and operation of the common areas of a development, where membership in the association is automatic with the purchase of a dwelling unit or lot within the development, with the ability to legally assess each owner of a dwelling unit or lot and which has authority to place a lien against all dwelling units and lots within the development.

Land suitability analysis: A method used by land planners, in preparing land use plans at a community-wide scale or land plans at a site development scale, to evaluate the fitness of land for various uses based at least partially on environmental criteria. The end product of land suitability analysis is typically a map or set of maps depicting the appropriateness of land areas for various land uses.

Lot: A portion or parcel of land separated from other portions or parcels by description (such as on a subdivision plat of record or a survey map or plat) or described by metes and bounds, and intended for use, transfer of ownership, or for building development. The word "lot" shall not include any portion of a dedicated right-of-way. Types of lots are illustrated in Figure 1-1-3.4.

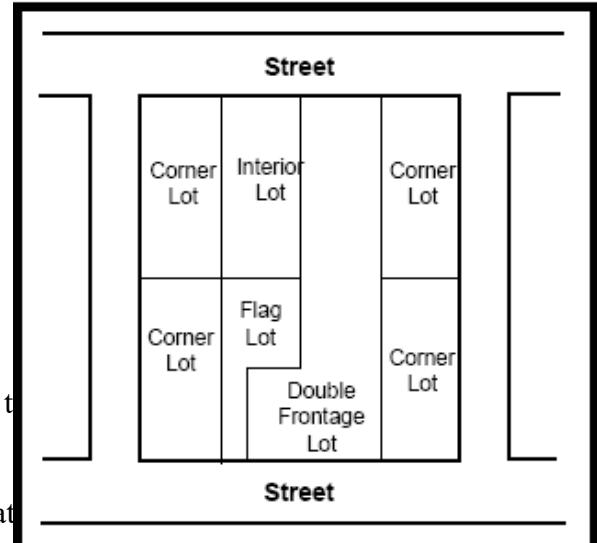
Lot, corner: A lot abutting upon two or more streets at their intersection.

Lot, depth: The average horizontal distance between the front and rear property lines of a lot, illustrated in Figure 1-1-3.5.

Lot, double frontage: A lot other than a corner lot that is not a corner lot and that does not intersect at a point abutting the property.

Lot, flag: A tract or lot of land of uneven dimensions in which the portion fronting on a street is less than the required minimum width required for construction of a building or structure on that lot. See Figure 1-1-3.4 for an illustration of a flag lot.

**Figure 1-1-3.4**  
Types of Lots

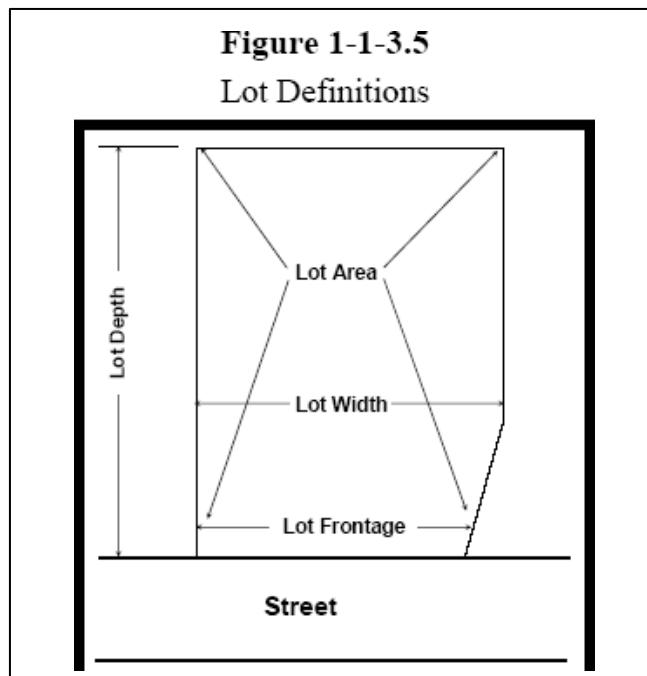


Lot, through: See “Lot, double frontage”.

Lot frontage: The width in linear feet of a lot where it abuts the right-of-way of any street. Lot frontage is illustrated in Figure 1-1-3.5.

Lot of record: A lot which is part of a subdivision approved in accordance with land subdivision requirements, a plat of which has been lawfully recorded in the records of the Clerk of the Bartow County Superior Court; or a parcel of land, the deed of which was lawfully recorded in the same office prior to the adoption date of this ordinance by the Mayor and City Council of Euharlee.

Lot width: The shortest distance between side lot lines measured at the regulatory/required front building line, or in the absence of a front building line regulation, the distance between side lot lines measured at the front line of the building located or intended to be located on the lot. Lot width is illustrated in Figure 1-1-3.5.



Natural conditions: The flora, fauna, soil and water conditions that would develop on a specific tract of land if all human interference were to be removed. The tract of land must have been undisturbed for a sufficient period of time for natural processes to dominate the tract. This period of time will vary among environments.

Off-site: Beyond the boundaries of the property in question.

On-site: Within the boundaries of the property in question.

Open space: Any combination of primary conservation areas and secondary conservation areas, as defined, that together form a permanent, undivided or relatively undivided,

undeveloped area. As much as 25 percent of the open space may be devoted to active recreational facilities, as defined. Easements for electric and gas transmission lines shall not be considered open space. Stormwater management features, such as lakes, ponds, and ways, may be considered open space at the discretion of the Zoning Administrator, provided that such areas are designed and maintained in a manner that contributes to open space and the aesthetics of the subdivision.

Open space, public: An area within a development or subdivision designed and intended for the use and enjoyment of all residents or for the use and enjoyment of the public in general.

Original tract: A unit of land which the owner holds under single or unified ownership, or which the owner holds controlling interest on the effective date of this Ordinance, where all land abutting said tract is separately owned by others, not related to or associated by business partnership with the owner.

Package treatment plant: A sewage treatment facility, usually privately operated, typically having a treatment capacity of less than one million gallons per day. In most cases, a package treatment plant is considered a temporary means of wastewater treatment until connection to a public sanitary sewerage system is available.

Pedestrian way: A public right-of-way or private easement across a block or within a block to provide access for pedestrians and which may, in addition to providing pedestrian access, be used for the installation of utility lines.

Performance bond: A type of subdivision improvement guarantee in the form of a bond, secured by the subdivider from a bonding company, in an amount specified by the City to cover the costs of required improvements, and payable to the City. The City may call in the performance bond in the event the subdivider defaults on required improvements.

Person: A natural human being, estate, association, firm, partnership, corporation, or other legal entity.

Preliminary plat: A tentative drawing or map of a proposed subdivision. A preliminary plat is the basis for the approval or disapproval of the general layout of a land subdivision.

Planned unit development: A form of development usually characterized by a unified site design for a number of housing units, clustered buildings, common open space, and a mix of building types and land uses in a slightly more dense setting than allowable on separate zoned lots.

Professional engineer: An engineer duly registered or otherwise authorized by the State of Georgia to practice in the field of civil engineering.

Professional surveyor: A surveyor duly registered or otherwise authorized by the State of Georgia to practice in the field of land surveying.

Protective covenants: Contracts made between private parties as to the manner in which land may be used, with the view toward protecting and preserving the physical and economic integrity of any given area.

Recreation, active: Leisure activities that are facility oriented, such as swimming pools, tennis courts, and ball fields.

Recreation, passive: Leisure activities that are natural resource oriented, such as hiking trails, conservation areas, and nature preserves.

Reservation: A method of holding land for future public use by showing proposed public areas on a subdivision plat.

Reserve strip: A strip of land across the end of, or along the edge of, a street, alley, or lot for the purpose of controlling access which is reserved or held until future street extension or widening.

Right-of-way:

- (a) A strip of land acquired by reservation, dedication, forced dedication, prescription, or condemnation and intended to be occupied by a road, crosswalk, railroad, electric transmission line, oil or gas pipeline, water line, sanitary storm sewer, or other similar use.
- (b) Generally, the right of one to pass over the property of another.

Scenic views and sites: Those geographic areas containing visually significant or unique natural features, as identified in the Comprehensive Plan, or by other reasonable means.

Sensitive natural areas: Any area, as identified now or hereafter by the Department of Natural Resources, which contains one or more of the following:

- (a) Habitat, including nesting sites, occupied by rare or endangered species;
- (b) Rare or exemplary natural communities;
- (c) Significant landforms, hydroforms, or geological features; and/or
- (d) Other areas so designated by the Department of Natural Resources which are sensitive or vulnerable to physical or biological alteration.

Septic tank: An approved watertight tank designed or used to receive sewage from a building sewer and to affect separation and organic decomposition of sewerage solids, and discharging sewage effluent to an absorption field or other management system.

Shade tree: A tree in a public place, street right-of-way, or special easement, planted to provide canopy that will obscure the sun and heat from the ground.

Sidewalk: A hard-surfaced pedestrian access area adjacent to or within the right-of-way of a public road.

Site plan: A neat and approximate drawing of a multi-family residential, institutional, office, commercial, or industrial development, showing the general layout of a proposed

development including among other features the location of buildings, parking areas, and buffers and landscaping. The site plan is the basis for the approval or disapproval of the general layout of a development in the case of a multiple-family residential, institutional, office, commercial, or industrial development.

Slope: Degree of deviation of a surface from the horizontal, usually expressed in percent or degree; the ratio of the difference in elevation between two points on the ground, and the horizontal distance between these two points. For purposes of determining steep slopes, slopes shall be measured between two points on the ground separated by 500 feet or more.

Steep slopes: Lands with slopes of at least 35 percent, as indicated in the Comprehensive Plan of the City, or which can be calculated with aid of a United States Geological Survey 1:24,000, 7.5 minute quadrangle topographic map or other available topographic information.

Street: Any vehicular way, other than an alley, that:

- (a) is an existing federal, state, county or municipal roadway;
- (b) is constructed as shown upon a plat approved pursuant to law and is open to vehicle travel;
- (c) is constructed and open to vehicle travel as approved by other official action of the Mayor and City Council; or
- (d) is constructed and open to vehicle travel and shown on a plat duly filed and recorded in the Clerk's Office, Bartow County Superior Court prior to the effective date of this Ordinance. Land between the street lines, whether improved or unimproved, shall be considered part of the street.

Street, collector: Unless otherwise defined by the Major Transportation Plan or Comprehensive Plan, a collector street is a public street whose function is to collect traffic from neighborhoods and local streets and which connects to another public street of equal or greater classification. A collector also may provide direct access to adjacent properties.

Street, local: Unless otherwise defined in the Major Transportation Plan or Comprehensive Plan, any public street, except an alley, collector, or arterial, and which has a primary function to provide direct access to adjoining properties and which serves a limited area only, usually a single land subdivision.

Street, major arterial: Unless otherwise defined by the Major Transportation Plan or Comprehensive Plan, a major arterial street is a street connecting two or more towns or communities, connecting two highways of equal or greater capacity, or serving as the primary access to large land areas.

Street, marginal access: A residential street parallel and adjacent to a major thoroughfare and which provides access to abutting properties with protection from through-traffic.

Street, minor arterial: Unless otherwise defined by the Major Transportation Plan or Comprehensive Plan, a minor arterial street is a street connecting two or more major areas of the City or serves as the primary access to a large land area. A minor arterial may also serve a

large traffic generator (e.g., an industrial area) and perform a secondary function of providing local access.

Street, private: A road or street that has not been accepted for maintenance by the City and that is not owned and maintained by a state, county, city, or another public entity.

Subdivider: Any person, as defined by this Ordinance, who undertakes the subdivision of land, and any person having such a proprietary interest in land to be subdivided as will authorize the maintenance of proceedings to subdivide such land under this Ordinance, or the authorized agent of such person.

Subdivision: A division of a tract or parcel of land into two or more lots, building sites, or other divisions for the purpose of sale or building development, whether immediate or future, including all division of land involving the dedication of a new street or a change in existing streets. The word “subdivision” includes re-subdivision and, when appropriate to the context, relates either to the process of subdividing or to the actual land or area which is subdivided.

Utility: Public or private water or sewer piping systems, water or sewer pumping stations, electric power lines, fuel or gas pipelines, telephone lines, roads, cable telephone line, fiber optic cable, driveways, bridges, river/lake access facilities, stormwater systems and drainage ways, and railroads or other utilities identified by the City. As appropriate to the context, the term “utility” may also include all persons, companies, or governmental agencies supplying the same.

Variance: A grant of relief from the strict requirements of this Ordinance which permits construction in a manner that would otherwise be prohibited by this Code; a minimal relaxation or modification of the strict terms of this Ordinance as applied to specific property when, because of particular physical surroundings, shape, or topographical condition of the property, compliance would result in practical difficulty.

#### §1-1-4 AUTHORITY AND DELEGATION

§1-1-4.1 Authority. These regulations are adopted pursuant to powers vested in cities by the State of Georgia Constitution, home rule powers, state administrative rules for the adoption and implementation of Comprehensive Plans, and the protection of vital areas of the State.

§1-1-4.2 Delegation of Powers to Zoning Administrator. The Zoning Administrator of Euharlee shall be charged with enforcing, administering and interpreting this Ordinance, with the advice of the City Engineer. The Zoning Administrator is vested with the authority to do the following:

- (a) administer this Ordinance, including issuing forms, reviewing applications for completeness, meeting with applicants regarding the requirements herein, and similar matters;

- (b) interpret this Ordinance when any questions as to the meaning or effect of any provision arises;
- (c) enforce this ordinance as specified in § 1-1-4.5 below;
- (d) review and recommend approval, conditional approval, or denial of preliminary plats, construction plans, and final plats; and
- (e) grant administrative variances from the requirements of this Ordinance, as specified in § 1-1-4.6 below. *Amended September 20, 2006.*

§1-1-4.3 Delegation of Powers to City Engineer. The City Engineer is vested with the authority to make recommendations to the Zoning Administrator and the Mayor and City Council on requirements for land development improvements and to make requirement recommendations for improvement guarantees for public improvements as specified in this Ordinance.

§1-1-4.4 Delegation of Powers to Hearing Examiner. The Euharlee Hearing Examiner, as established in the Euharlee Zoning Ordinance, Article XIV, shall be empowered to grant variances from this Ordinance and shall hear appeals from interpretations of the Zoning Administrator or City Engineer under this Ordinance.

*Amended September 20<sup>th</sup>, 2006.*

§1-1-4.5 Enforcement of Ordinance. The Zoning Administrator shall be empowered to enforce this Ordinance by means of citation to magistrate court, civil administrative fine, civil action in superior court, and all other means as detailed in the Zoning Ordinance of Euharlee, Section 13.5, which section is incorporated herein by reference, including minimum fines, except as modified by replacing references to the Zoning Ordinance with references to this ordinance, as applicable. *Amended September 20<sup>th</sup>, 2006.*

§1-1-4.6 Administrative Variances. The Zoning Administrator is empowered to grant administrative variances. An administrative variance may only be granted if, in the judgment of the Zoning Administrator, all the following criteria are met:

- 1) the standard or requirement of this Ordinance causes undue hardship on the applicant,
- 2) the hardship is related to topographic or other conditions of the land (such as size or shape), that were not created by the applicant's actions or otherwise by illegal act of a predecessor in title;
- 3) that the sole hardship on the applicant is not financial (that is, that the reason for the variance is only to reduce costs); and
- 4) that granting the variance would not impair the purposes of the ordinance, nor result in a negative impact on the surrounding uses, properties or neighbors.

If all the foregoing conditions are met, the Zoning Administrator shall be empowered to grant an administrative variance to alter any numeric or quantitative requirement by up to twenty (20) percent. The Zoning Administrator shall also be authorized to grant an administrative variance to other requirements of this Ordinance relating to design requirements for subdivisions and

infrastructure that are not quantitative, if the above criteria are met, and the Zoning Administrator further determines that such variance is minor and better serves the purposes and intent of this Ordinance than the existing regulation, under the particular circumstances at issue. The variance granted should be the minimum that will make possible the development. The Zoning Administrator may consult with the City Engineer regarding the impact of the proposed request.

All administrative variances shall be granted in writing, with a copy to the file and the applicant. In granting an administrative variance, the Zoning Administrator may attach such conditions as the Zoning Administrator may deem advisable so that the purpose of this Ordinance will be served, public safety and welfare secured, and substantial justice done. The decision to grant an administrative variance or deny an administrative variance may be appealed by any aggrieved party to the Hearing Examiner.

*Amended September 20<sup>th</sup>, 2006.*

## §1-1-5 APPLICABILITY AND GENERAL PROVISIONS

- §1-1-5.1 Applicability. These regulations shall apply to all real property within corporate limits of the City of Euharlee.
- §1-1-5.2 Land is One Tract Until Subdivided. Until property proposed for subdivision has received final plat approval and been properly recorded, the land involving the subdivision shall be considered as one tract, or as otherwise legally recorded.
- §1-1-5.3 All Land Subdivisions to Comply. No person shall sell, advertise, or offer to sell, by deed, map, plat or other instrument, any parcel of land not subdivided under the requirements of this Ordinance. It shall be unlawful for any person to transfer or sell land by reference to, or by exhibition of, or by other use of, a plat of a land subdivision that has not been approved and recorded in accordance with the requirements of this Ordinance. The description of such land by metes and bounds in the instrument of transfer shall not exempt the transaction. No plat of land subdivision shall be entitled to be recorded in the Office of the Clerk of the Superior Court of Bartow County, and it shall be unlawful to record such a plat of land subdivision, unless and until it shall have been approved in accordance with the requirements of this Ordinance.
- §1-1-5.4 Preliminary Plat and Plans Required Prior to Construction. No person shall commence construction of any improvements on any lot, prior to the approval of a preliminary plat if required by this Ordinance, nor prior to approval of construction plans and engineering plans for said improvements are approved as required by this Ordinance and Section 1-2 of this code.
- §1-1-5.5 Building and Other Permits. No building permit shall be issued for a building, structure, or use, nor shall any excavation, grading, or land disturbance

applications be approved, on any parcel of land regulated by this Ordinance that has not been approved in accordance with the provisions of this Ordinance.

§1-1-5.6 Public Streets and Lands. No land dedicated as a public street or for other public purpose shall be opened, extended, or accepted as a public street or for other public land unless such improvements are constructed in accordance with the specifications of this Ordinance and said land and/or improvements are formally approved and accepted as public improvements by the Mayor and City Council in accordance with procedures established in this Ordinance.

§1-1-5.7 Appeals. Any person aggrieved by an interpretation or decision of the Zoning Administrator, City Engineer, or other official responsible for the administration of this Ordinance (other than a decision to issue a citation, but including a decision to issue or deny an administrative variance) may file an appeal to the Hearing Examiner in accordance with Section 14.6 of the City of Euharlee Zoning Ordinance, which section is incorporated herein by reference, except as modified by replacing references to the Zoning Ordinance with references to this ordinance, as applicable. Decisions of the Hearing Examiner regarding appeals may be appealed to superior court as specified in the Zoning Ordinance, Sec. 14.12.

Decisions of the City Council may not be appealed to the Hearing Examiner. Decisions of the City Council may only be appealed by writ of certiorari to the Superior Court filed within thirty days following the procedure of the Georgia Code, Title 5. Appeals of decisions of the City Council shall be on a review of the record of the hearing, supplemented only by argument of counsel. *Amended September 20<sup>th</sup>, 2006.*

§1-1-5.8 Variances. The Hearing Examiner is empowered to grant variances that cannot be granted administratively by the Zoning Administrator. Such variances may be granted relating to quantitative and dimensional requirements of this ordinance, or relating to design requirements for subdivisions and infrastructure that are not quantitative. The applicant shall file a variance application in accordance with the procedures of Section 14.7 of the Euharlee Zoning Ordinance. The variance shall be processed, reviewed and granted or denied under the procedures and standards of Article XIV of the zoning ordinance, which Article is incorporated herein by reference, except as modified by replacing references to the Zoning Ordinance with references to this ordinance, as applicable. In addition to the requirements and standards contained therein, it is the policy of the City that no variance may be granted that impairs the purposes of this ordinance, endangers the public health, safety or welfare, or has a negative impact on surrounding uses, properties or neighbors. Given the technical nature of subdivision plans, the Hearing Examiner may request the City Engineer or Zoning Administrator to render an opinion to assist in his decision, which opinion shall be a part of the record. Decisions of the Hearing Examiner regarding variances may be appealed to superior court as specified in the Zoning Ordinance, Sec. 14.12.

*Amended September 20<sup>th</sup>, 2006.*

## 1-1-6 EXEMPTIONS FROM PLAT APPROVAL

The following types of land subdivisions, transfers, and sales are specifically exempted from the plat approval requirements of this Ordinance;

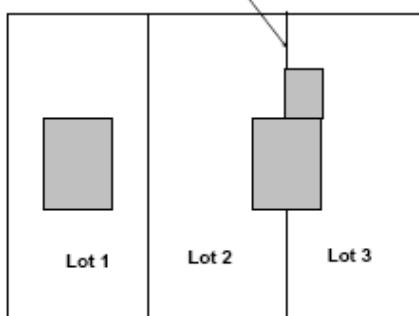
- (a) The creation and sale of cemetery plots.
- (b) The sale of lots consistent with previously approved and recorded plats or deeds.
- (c) The creation of leaseholds for space within a multiple-occupancy building or the division of property into leaseholds for commercial, industrial, or institutional use.
- (d) The creation of leaseholds for the agricultural use of property where the use does not involve the construction of a building to be used as a residence or for other purposes not directly related to agricultural use of the land or crops or livestock raised thereon.
- (e) Any division of land to heirs through a judicial estate proceeding, or any division of land pursuant to a judicial partition, or any division of land occurring from the foreclosure of a deed of trust; provided, however, that such exemption shall not require the City to issue permits if the resulting lots or parcels fail to meet any applicable regulations of the local jurisdiction.
- (f) The transfer of property by the owner to a member of the immediate family within the third degree, provided that no portion is less than one (1) acres.

## 1-1-7 LOT COMBINATIONS

An existing lot line forming the boundary between two conforming platted lots located within the same subdivision or a lot line between lots or parcels that have merged to form one building lot may be removed or eliminated through a final plat revision process which conforms to the requirements of this Ordinance. In the case no final plat applies to the subject lots or parcels, a boundary survey and plat depicting all lots involved in the lot combination shall be required to be approved by the Zoning Administrator and City Engineer and recorded as a final plat. Such combination plat shall be titled with the same name as that of the original subdivision, if applicable, and shall indicate thereon that the replat is for the purpose of removing the lot lines between specific lots. (See Figure 1-1-7.1).

**Figure 1-1-7.1**  
**Lot Combinations**

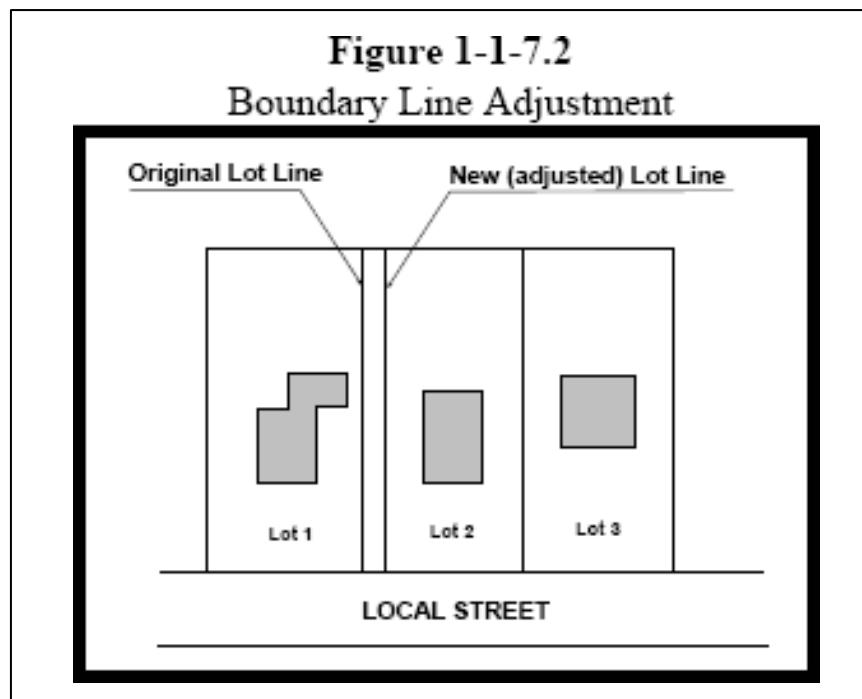
Elimination of  
platted lot line



**LOCAL STREET**

## §1-1-8 BOUNDARY LINE ADJUSTMENTS

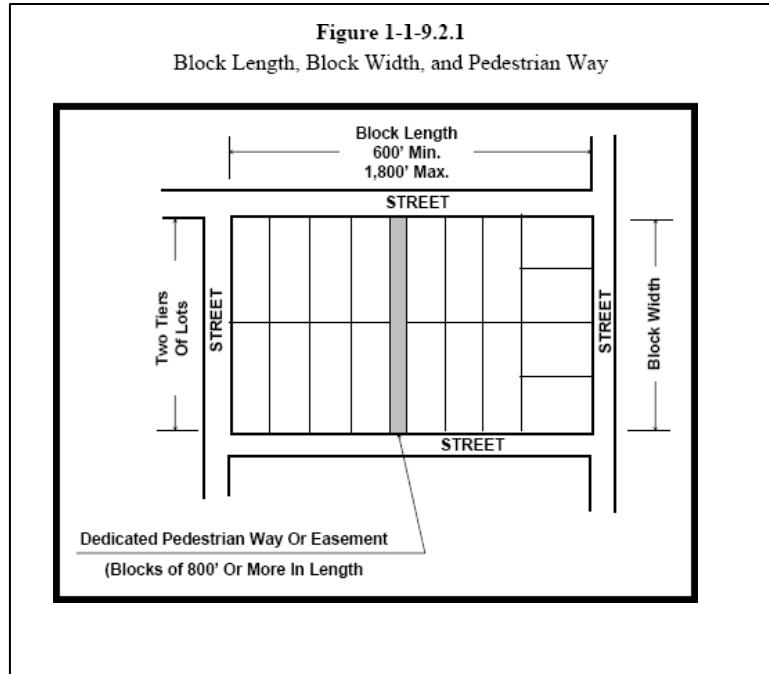
One or more existing lot lines forming boundaries between conforming platted lots located within the same subdivision, or one or more lot lines between abutting lots or parcels may be adjusted through a final plat revision process that requires the approval of the Director and recording of a plat meeting the specifications of a final plat. In the case no final plat applies to the subject lots or parcels, a boundary survey and plat of the entire lots involved in the boundary line adjustment shall be required to be approved by the Director and recorded. Such plat showing said boundary line adjustment shall be titled with the same name as that of the original subdivision and shall include thereon that the replat is for the purpose of adjusting the lot lines between specific lots. (See Figure 1-1-7.2)



## §1-1-9 DESIGN REQUIREMENTS FOR BLOCKS

§1-1-9.1 Block Length. Intersecting streets shall be provided at such intervals so as to provide adequate cross traffic. Blocks in residential subdivisions should not exceed 1,800 feet nor be less than 600 feet in length, except where topography or other conditions justify a departure from these standards. The Director and City Engineer may require pedestrian ways and/or easements through the block be located near the center in blocks longer than 800 feet.

§1-1-9.2 Block Width. The width of the block shall normally be sufficient to allow two tiers of lots of appropriate depth. Blocks intended for business or industrial use shall be of such width as to be considered most suitable for their respective use, including adequate space for off-street parking and deliveries. (See Figure 1-1-9.2.1).



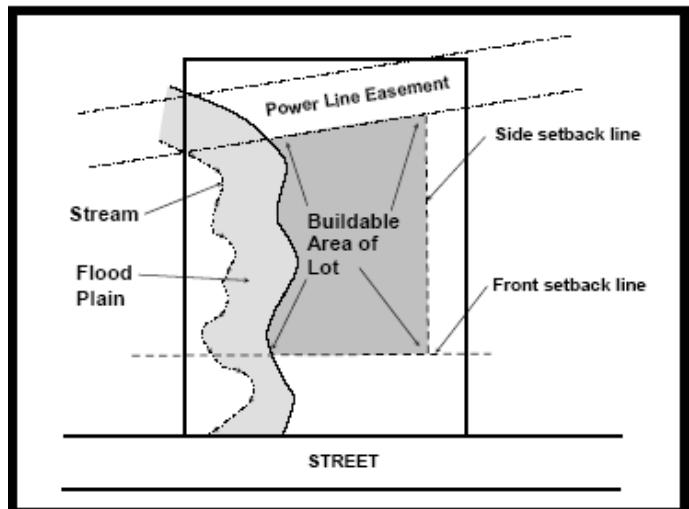
## §1-1-10 DESIGN REQUIREMENTS FOR LOTS

§1-1-10.1 Natural Features and Assets. In the subdividing of land, due regard shall be shown for all natural features, such as tree growth, watercourses, historic sites or similar conditions which, if preserved, will add attractiveness to the proposed development and safety from hazards.

§1-1-10.2 Access and Minimum Lot Frontage. Each lot shall have access to a public street and a minimum of 55 feet of lot frontage at the building line and the building line shall be no more than 100 feet from the right of way; provided, however, that the City may permit one or more lots to be accessed by private streets, as more fully specified in Section 1-1-19 of this code; provided further, that in the case of a lot accessed by a circular cul-de-sac, the minimum lot frontage may be reduced to 30 feet measured along the arc. See Section 8.1 of the Zoning Ordinance for minimums for specific zoning categories.

§1-1-10.3 Adequate Buildable Area Required. Land subject to flooding, improper drainage or erosion, or that is unsuitable for residential or other use for topographical or other reasons, shall not be platted for residential use nor for any other use that will continue or increase the danger to health, safety, or of property destruction, unless the hazards can be and are corrected. Each lot shall contain an adequate building site not subject to flooding and outside the limits of any existing easements or building setback lines required by the local governing body. (See Figure 1-1-10.3.1).

**Figure 1-1-10.3.1**  
Adequate Building Area



§1-1-10.4 **Lot Remnants Not Permitted.** All remnants of lots below any required minimum lot size that may be required, left over after subdividing of a larger tract, must be added to adjacent lots, rather than allowed to remain as unusable parcels. A lot remnant may be permitted for a specific purpose such as a detention pond, provided that access and design is appropriate and the lot remnant is restricted to specific non-building use.

§1-1-10.5 **Service Areas.** Commercial and industrial lots shall be adequate to provide service areas and off-street parking suitable to the use intended.

§1-1-10.6 **Lot Area.** The minimum lot area shall not be less than that established by the land use intensity district in which the subdivision is located, if applicable.

§1-1-10.7 **Lot Width.** No portion of a lot shall be narrower than 55 feet, with the exception of cul-de-sac lots, nor shall any lot have a lot width less than that established by the land use intensity district in which the subdivision is located, if applicable.

§1-1-10.8 **Lot Depth.** Lots shall have a depth of not less than 100 feet, unless circumstances make these limitations impracticable.

§1-1-10.9 **Flag lots.** No lot shall be approved which constitutes a flag lot except with special approval from the Zoning Administrator due to extreme topographic circumstances.

§1-1-10.10 Side Lot Lines. Insofar as practical, side lot lines shall be at right angles to straight street lines or radial to curved street lines.

§1-1-10.11 Corner Lots. Corner lots shall have adequate width to meet the front building setback requirements, if applicable, from all rights-of-way.

§1-1-10.12 Double Frontage Lots. Double frontage or “through” lots should be avoided except where essential to provide separation of residential development from arterials or overcome specific disadvantages of topography or orientation. Double frontage lots with frontage on a major arterial street shall have additional depth in order to allow space for screen planting along the lot line abutting a major arterial street.

#### §1-1-11 EASEMENTS

Where a watercourse, drainage way, channel, or stream traverses a subdivision, there shall be provided a stormwater or drainage easement of adequate width. Easements shall be provided for all drainage facilities as approved by the City Engineer, and shall extend to the property line when off the street right-of-way. Where easements are needed for utility locations, the subdivider shall provide them to the appropriate utility provider. Where easements are needed for public water and/or sanitary sewer lines, they shall be provided as determined appropriate by the City Engineer. All easements required pursuant to this section shall be shown on the preliminary plat, if required, and final plat.

#### §1-1-12 SURVEY MONUMENTS FOR ALL LOTS REQUIRED

For all subdivisions, a Georgia registered land surveyor shall install permanent survey monuments at all property corners and land lot lines, prior to final plat approval. Lot corners shall be marked with metal rods not less than 1/2" in diameter and 18" in length and driven so as to be stabilized in the ground. Permanent survey monuments shall also be installed in accordance with the most recent edition of Section 180-7-05 Monument of the Rules of State Board of Registration for Professional Engineers & Land Surveyors and the Georgia Plat Act (O.C.G.A. 15-6-67).

#### §1-1-13 PRELIMINARY PLAT

§1-1-13.1 Purpose. The purpose of this section is to ensure compliance with the basic design concepts and improvement requirements of subdivisions and land developments through the submittal of a tentative map of all subdivisions for review and recommendations by the Zoning Administrator and City Engineer, Planning and Zoning Commission, and approval by the Mayor and City Council.

§1-1-13.2 When Required. All subdivisions shall require the submission of a preliminary plat to the Zoning Administrator and Planning and Zoning Commission for review and recommendations to the Mayor and City Council. Prior to the issuance of any permit for land disturbance, or the installation of any

improvements, the Zoning Administrator and Planning and Zoning must review and make recommendations to the Mayor and City Council on the preliminary plat. Final approval or denial of the preliminary plat will be by the Mayor and City Council.

§1-1-13.3 Preliminary Plat Application and Specifications. Preliminary plat applications shall be made in accordance with requirements shown in Table 1-1-15.1.1, and preliminary plats shall meet the minimum plat specifications shown in Table 1-1-15.2.2.

§1-1-13.4 Procedures. Upon receipt of a completed preliminary plat application, the Zoning Administrator shall schedule the application for the next public meeting before the Planning and Zoning Commission and forward all pertinent materials in the application to the Commission Members for review. An incomplete application shall not be accepted until it is complete. A complete application for preliminary plat review and recommendation to the City Council must be submitted at least 14 days prior to the Planning and Zoning meeting and 21 days before the regular meeting date of the City Council to be considered on that agenda. The Zoning Administrator shall make a recommendation regarding approval or denial of the preliminary plat, including recommendation of any appropriate conditions. The basis of the Zoning Administrator's review of and recommendation on a preliminary plat shall be whether the preliminary plat meets the purposes and requirements of this Ordinance, and all other relevant ordinances that relate to the proposed development. The Zoning Administrator shall consult with the City Engineer as necessary in making their review. The Zoning Administrator's and Planning and Zoning Commission's recommendations may not be appealed, and the City Council can review an application without a recommendation..

§1-1-13.5 Decision by City Council. At the public meeting, the City Council shall take action on the application, which action can include approval, conditional approval, tabling, or denial of the preliminary plat application. The application can be tabled one month for submission of further information or to allow revision. If approved, the City Council is empowered to impose conditions on the approval that are relevant to this ordinance, relate to the public health, safety and welfare, and/or serve to ameliorate any negative impacts of the development on adjacent property. If an approval, conditional approval, or denial is not issued by the City Council within 60 days of the first public hearing on that application, the application shall be deemed approved. *Amended.*

§1-1-13.6 Limitations on Approval. Approval of a preliminary plat shall be valid for a period of one year. Preliminary plat approval shall expire and be null and void after a period of one year if construction plans have not been approved, unless the Zoning Administrator and the City Council approve an extension of time. Improvements cannot be commenced until construction plans are approved. In the event preliminary plat approval expires, the applicant shall be required to resubmit a preliminary plat application under the regulations as they exist at that time, not as they existed at the time of the original application.

§1-1-13.7 Amendments to Approved Preliminary Plats. All amendments to preliminary plats require the Zoning Administrator and City Engineer's review and recommendation. The Zoning Administrator shall recommend approval, conditionally approval, or denial of the proposed amendment to a preliminary plat. Procedures for considering an amendment to a preliminary plat shall be the same as required for an initial application for preliminary plat approval. Final approval, conditional approval or denial of a preliminary plat amendment shall be by decision of the City Council. Final plats shall be required to conform substantially to preliminary plats, and therefore any proposed significant change to the configuration of the streets, the number of lots, the provision of greenspace or open space, access points or other matters shall require an amendment to the preliminary plat be approved. *Amended September 20<sup>th</sup>, 2006.*

§1-1-13.8 Approval Certificate. Upon approval of the preliminary plat, a certificate, stamped directly on the preliminary plat, shall state:

"Preliminary Approval: \_\_\_\_\_  
\_\_\_\_\_  
Zoning Administrator (Date)

\_\_\_\_\_  
Mayor (Date)

This preliminary approval does not constitute approval of a final plat. This Certification of Preliminary Approval shall expire and be null and void on \_\_\_\_\_. "

## §1-1-14 CONSTRUCTION PLANS

§1-1-14.1 Application. The subdivider or land developer must submit construction plans in conjunction with the preliminary plat for approval. In cases where a preliminary plat is not required by this article, the subdivider or land developer may apply for approval of construction plans; provided, however, the applicant for construction plan approval should hold a pre-application conference with the Zoning Administrator to ensure that plans meet the intent and specific provisions of this Ordinance and other applicable regulations. Construction plans must be approved by the Zoning Administrator and the Mayor and City Council. Applications for construction plan approval shall be made in accordance with requirements shown in Table 1-1-15.1.1 and Table 1-1-15.2.1.

§1-1-14.2 Decision Criteria. The only basis upon which the Zoning Administrator and the Mayor and City Council may deny a construction plan is the failure of the application to meet the requirements of this Ordinance or any other applicable local regulations.

§1-1-14.3 Certificate of Approval. All copies of the construction plans shall be noted by inscription on the plat noting such approval by the City.

"Construction Plan Approval: \_\_\_\_\_  
Zoning Administrator \_\_\_\_\_ (Date) \_\_\_\_\_

---

Mayor \_\_\_\_\_ (Date) \_\_\_\_\_

This Certification of Construction Plan Approval shall expire and be null and void on \_\_\_\_\_.

Construction plan approval shall expire and be null and void after a period of one year, unless activity toward improvements on the land has been initiated, or unless the Zoning Administrator and the Mayor and City Council approves an extension of time. Construction plan approval shall expire and be null and void after a period of eighteen months, if improvements on the land have not been completed, or unless the Zoning Administrator and the Mayor and City Council approves an extension of time.

§1-1-15 FINAL PLAT

§1-1-15.1 When Required. All subdivisions and dedications shall require final plat approval. Final plat approval is by the Zoning Administrator and the Mayor and City Council. Applications shall be made in accordance with requirements shown in Table 1-1-15.1.1. Final plats shall be required to conform substantially to preliminary plats. *Amended September 20<sup>th</sup>, 2006.*

**Table 1-1-15.1.1**  
Application Requirements

	Preliminary Plat	Construction Plans	Final Plat
Pre-application review with staff	Recommended		
Application form completed	Required	Required	Required
Letter requesting approval with name, address, and phone of applicant	Required	Required	Required
Number of copies	5	5	5
Filing fee per schedule Ordinance/	Required	Required	Required
Description of type of water supply and sewerage system and utilities to be provided	Required	Required	Required
Soil test for each lot proposed for on-site septic tank and drain field	Required	Required	Required
Data on existing conditions	Required		
Hydrological or other engineering study		Required	
Subdivision entrance monument and landscaping elevation/plan (prepared by landscape architect)		Required	
Warranty deed for the dedication of streets and other public places			Required
Written approval from electric utility company regarding installation of service points and street lights			Required
As-built drawings of public improvements			Required
Subdivision improvement guarantee			Required
Certificate of title			Required
Plat Certificates			Required

**Table 1-1-15.2.1**  
**Preliminary Plat, Construction Plans and Final Plat Requirements**

<b>REQUIRED INFORMATION</b> (Required to be on the plat or construction plans)	<b>Preliminary Plat</b>	<b>Construction Plans</b>	<b>Final Plat</b>
Scale (minimum)	1"=100 feet	1"=100 feet	1"=100 feet
Sheet size (maximum)	24" x 36"	24" x 36"	18" x 22"
North arrow and graphic engineering scale	Required	Required	Required
Reference to north point (magnetic, true north, or grid north)	Required	Required	Required
Proposed name of subdivision or project and phases, if any	Required	Required	Required
Vicinity map With North Arrow	Required	Required	Required
Total acreage of the property being subdivided	Required	Required	Required
Name, address, and telephone of owner of record	Required	Required	Required
Name, address and telephone of subdivider	Required	Required	Required
Name, address and telephone of preparer of plat	Required	Required	Required
Date of plat drawing and revision date(s), if any	Required	Required	Required
Exact boundaries of the tract to be subdivided by bearings and distances, tied to one or more benchmarks	Required	Required	Required
Names of owners of record of all abutting land		Required	Required
Municipal, County and land lot lines inside the property or within 500 feet.	Required	Required	Required
Existing buildings and structures on or encroaching on the tract to be subdivided	Required	Required	Not Shown
Existing streets, utilities and easements on and adjacent to the tract	Required	Required	Required
Environmental conditions (streams, wetlands, watershed protection districts, flood hazard areas, river corridor boundaries, etc.)	Required	Required	Required
Block boundaries lettered and each lot numbered consecutively counterclockwise without repetition			Required
Dimensions and acreage of all lots	Approximate	Approximate	Exact
Locations of streets, alleys, lots, open spaces, and any public use reservations and/or common areas	Required	Required	Required
Right-of-way widths and pavement widths for existing and proposed streets		Required	Required
Locations, widths and purposes of easements		Required	Required
Street centerlines showing angles of deflection, angles of intersection, radii, and lengths of tangents and arcs, and degree of curvature and curve data		Required	Required
Acreage to be dedicated to the public	Approximate	Approximate	Exact
Street names	Recommended	Required	Required
Street mailing address for each lot			Required
Topography		Required	Not Shown
Minimum front building setback lines for all lots	Required	Required	Required
Location and description of all monuments			Required
Certificate of ownership and dedication			Required
Plat recording and signature block			Required
Signature block for Planning and Zoning Commission approval	Required	Required	Required
Signature block for Mayor	Required	Required	Required
Land surveyor's stamp, certificate, signature, including field survey and closure statement		Required	Required
Statement of and reference to private covenants		Recommended	Required
Schedule of construction for all proposed projects with particular attention to development planned for the first year	Required	Required	

§1-1-15.2 Criteria for Approval. Final plat approval may be granted if the following conditions, as applicable, are met. (See Table 1-1-15.2.1).

- (a) The Mayor and City Council has previously approved a preliminary plat of the proposed subdivision, if required.
- (b) Where new improvements are involved in the subdivision, construction plans have been approved by the Mayor and City Council, and all improvements have been installed and inspected by the City, and subdivision improvement guarantees as required by this Ordinance have been submitted.
- (c) The final plat meets all applicable requirements of this Ordinance.
- (d) A complete final plat application has been submitted, including all supporting materials required by this chapter for final plats.

Denial of a final plat shall be permitted only upon specific findings that one or more of the above-referenced conditions have not been met. (See Table 1-1-15.2.1).

§1-1-15.3 Approval Certificate. Upon approval of the final plat, a certificate, stamped directly on the plat, shall state:

"Pursuant to the Land Subdivision Regulations of the City of Euharlee, Georgia, and all requirements of approval having been fulfilled, this final plat was given approval by Zoning Administrator on \_\_\_\_\_, 20\_\_\_\_\_, and final approval Mayor and City Council on \_\_\_\_\_, 20\_\_\_\_\_, and it is entitled to recordation in the Clerk's Office, Bartow County Superior Court."

\_\_\_\_\_  
Zoning Administrator

\_\_\_\_\_  
Mayor

§1-1-15.4 Additional Plat Certificates. In addition to information required by Table 1-1-15.2.1 to be supplied on a final plat, each final plat shall contain the following certificates. Surveyor's Certificate. A certificate by a surveyor directly on the final plat as follows:

"It is hereby certified that this plat is true and correct and was prepared from an actual survey of the property by me or persons under my supervision; that all monuments shown hereon actually exist or are marked as "future," and that their location, size, type and material are correctly shown; and that all engineering requirements of the City of Euharlee, Georgia, have been fully complied with.

By: \_\_\_\_\_

Registered Georgia Land Surveyor No.: \_\_\_\_\_"

Owner's Certificate. A certificate by the owner directly on the final plat, signed in an appropriate manner as follows:

"The owner of the land shown on this plat and whose name is subscribed hereto, in person or through a duly authorized agent, certifies that this plat was made from an actual survey, and that all State, City and County taxes or other assessments now due on this land have been paid. Said owner donates and dedicates to the public for use forever the street right-of-way as shown on this plat.

---

Owner

Signed, sealed and delivered  
in the presence of:

---

Witness

---

Notary Public"

---

**Health Department Approval Certificate.**

"This final plat has been approved by the Bartow County Health Department as being consistent with applicable state and local environmental health requirements.

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Director, Bartow County Health Department"

**§1-1-16 DEDICATIONS OF STREETS AND PUBLIC LANDS**

Subdivision streets and rights-of-way and other lands to be dedicated to the public shall be accepted and dedicated by the City only upon the delivery to the Mayor and City Council of the general warranty deed conveying fee simple title of such rights-of-way and lands. The warranty deed shall be accompanied by an attorney's certificate of title and a tax transfer form addressed to the Mayor and City Council certifying that the grantor in such deed is vested with marketable fee simple title to the property conveyed thereby, free and clear of all liens and encumbrances, and further that the individual executing such deed has full authority to do so. Acceptance of such dedication shall be accomplished by Resolution of the Mayor and City Council, a certified copy of which shall be attached to both the deed of dedication and the final plat.

**§1-1-17 SUBDIVISION IMPROVEMENT GUARANTEES**

In order to protect the City and prospective purchasers of and residents in a subdivision, the subdivider/developer shall provide to the City financial security to guarantee the installation of public improvements. The subdivider's or developer's financial guarantee may be any of the following:

- (a) An escrow of funds with the City;
- (b) An escrow with a bank or savings and loan association upon which the City can draw;

- (c) A performance bond for the benefit of the City upon which the City can collect, or a certificate of deposit with assignment letter. The bond term shall be five years or until 80% of the construction is completed; and
- (d) Any other form of guarantee approved by the Mayor and City Council that will satisfy the objectives of this section. The guarantee shall be in an amount to secure the full costs, as determined by the City, of constructing or installing the improvements and utilities required.

## §1-1-18 PRIVATE STREETS

§1-1-18.1 Private Streets Permitted. Private streets may, upon application, be permitted by the Mayor and City Council within major subdivisions, subject to the requirements of this section. Applications for approval of private streets shall be considered by the Mayor and City Council at the time of preliminary plat approval by the Zoning Administrator. Following a recommendation by the Zoning Administrator to authorize private streets in a major subdivision, the Mayor and City Council shall consider the application and may impose conditions on the approval of private streets to ensure various public purposes and to mitigate potential problems with private streets. No final plat involving a private street shall be approved unless said final plat conforms to the requirements of this section.

§1-1-18.2 Engineering Plans Required. It shall be unlawful for any person, firm, or corporation to construct a new private street or alter an existing private street or to cause the same to be done without first obtaining approval of engineering and construction plans from the Zoning Administrator and Mayor and City Council in accordance with the requirements of this Ordinance and Section 1.2 of this code.

§1-1-18.3 Standards. All private streets shall be constructed to all standards for public streets as required by Section 1.2 of this code, applicable construction specifications of the City Engineer, and as approved by the Zoning Administrator and Mayor and City Council.

§1-1-18.4 Street Names And Signs. Private streets shall be named, subject to the approval of the Zoning Administrator. The subdivider of land involving a private street shall install street signs with content containing the street name and the designation “private,” as approved by the Administrator. The sign signifying the private street may be required by the Zoning Administrator to be a different color than that of street signs provided for public streets, in order to distinguish maintenance responsibilities in the field. All signage must meet Manual of Uniform Traffic Control Devices – Georgia specifications.

§1-1-18.5 Easements. Easements for private streets shall be designated on final plats as general-purpose public access and utility easements, along with the name of said private street. Said easement shall at minimum be of the same width as that required for the right-of-way of a public street by the major thoroughfare plan and the City for the type of public street (local, collector, etc.) most closely resembling the proposed private street. Easements for private streets shall not be

included in any calculation of minimum lot size or density limitations established by local land use regulations. In the cases of private streets, the general-purpose public access and utility easement for the private street shall either;

- (a) Be shown in a manner on the final plat such that each lot fronting the private street extends to the centerline of the private street. No lot shall be permitted to be divided by the general-purpose public access and utility easement required and established for a private street; or
- (b) Shall be drawn as its own discrete parcel to be dedicated to a private homeowner's association (i.e., not shown to be a part of any lot).

**§1-1-18.6 Maintenance**. The City shall not maintain, repair, resurface, rebuild, or otherwise improve streets, signs, drainage improvements or any other appurtenances within general purpose public access and utility easements established for private streets. A private maintenance covenant recorded with the County Clerk of the Superior Court shall be required for any private street and other improvements within general purpose public access and utility easements established for private streets. The covenant shall set out the distribution of expenses, remedies for non-compliance with the terms of the agreement, rights to the use of easements, and other pertinent considerations. The Covenant shall specifically include the following terms.

- (a) The Covenant shall establish minimum annual assessments in an amount adequate to defray costs of ordinary maintenance and procedures for approval of additional needed assessments. The Covenant shall also specify that the funds from such assessments will be held by a homeowners or property owners association in cases of a subdivision of seven or more lots fronting on a private street.
- (b) The Covenant shall include a periodic maintenance schedule.
- (c) The Covenant for maintenance shall be enforceable by any property owner served by the private street.
- (d) The Covenant shall establish a formula for assessing maintenance and repair costs equitably to property owners served by the private street.
- (e) The Covenant shall run with the land.
- (f) The Mayor and City Council may, at its discretion, as a condition of approving private streets, require a performance bond and/or maintenance bond be submitted by the subdivider and held by a homeowners or property owners association, or the Mayor and City Council may require that the subdivider pay an amount of money as recommended by the City Engineer into an escrow account or other suitable account for the maintenance and repair of private streets and stormwater management improvements, to be drawn from by the homeowners or property owners association as maintenance and repair needs may arise.

**§1-1-18.7 Specifications For Final Plats Involving Private Streets**. The Zoning Administrator shall not approve for recording any final plat involving a private street unless and until it shall contain the following on the face of the plat:

Deed book and page reference to the recorded covenant required by this section; **“WARNING, City of Euharlee has no responsibility to build, improve, maintain, or otherwise service the private streets, drainage improvements, and other appurtenances contained within the general public purpose access**

**and utility easement or easements for private streets shown on this plat.”;**

(a) **“Grant of Easement. The general purpose public access and utility easement(s) shown on this plat for private street(s) is hereby granted and said grant of rights shall be liberally construed to provide all necessary authority to the City, and to public or private utility companies serving the subdivision, for the installation and maintenance of utilities, including, but not limited to, electric lines, gas lines, telephone lines, water lines, sewer lines, cable television lines, and fiber optic cables, together with the right to trim interfering trees and brush, together with a perpetual right of ingress and egress for installation, maintenance, and replacement of such lines.**

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**Signature of Property Owner”;** and,

(b) The following certificate of dedication shall be required, unless the Mayor and City Council waives the dedication requirement.

**“Certificate of Dedication. All water and sewer lines installed within the general purpose public access and utility easement(s) shown on this plat for private street(s) are hereby dedicated to City of Euharlee.**

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**Signature of Property Owner.”**

**§1-1-18.8 Requirement for Purchaser’s Acknowledgement of Private Responsibilities.** Prior to the sale or as a condition of the closing of a real estate transaction involving any lot served by a private street in the city, the subdivider or seller of said lot shall execute a notarized purchaser’s acknowledgement of private street construction and drainage maintenance responsibilities as set forth below. A copy of the purchaser’s acknowledgement shall be retained by the purchaser and shall be required to be submitted as a condition of a building permit for a principal building on said lot:

**“Purchaser’s Acknowledgement of  
Private Street and Drainage Maintenance Responsibility**

**(I) / (We) have read the Declaration of Covenant which pertains to the lot that is the subject of this real estate transaction \_\_\_\_\_ (insert address or attach legal description). (I) / (We) understand that the Declaration of Covenant applies to the lot that (I am) / (we are) purchasing and requires (me)**

**/ (us) to provide a specified percentage or amount of the financing for the construction and maintenance of any private street and drainage facilities serving the lot which (I am) / (we are) purchasing, and that owners of other lots in this plat may sue for and recover those costs which this covenant requires (me) / (us) to pay, plus their damages resulting from (my) / (our) refusal to contribute, plus reasonable attorney’s fees. (I) / (We) furtherunderstand that the City has no obligation to assist with the maintenance and improvement of the private street, drainage facilities, and other appurtenances within the general purpose public access and utility easement for the private road serving the lot in question. (I) / (We) understand that a copy of this purchaser’s acknowledgement shall be required as a condition of the issuance of a building permit for a principal building on the**

lot (I am) / (we are) purchasing.

Purchaser

Purchaser.”

## **§1-2 IMPROVEMENTS REQUIRED FOR SUBDIVISIONS AND LAND DEVELOPMENT**

### **§1-2-1 PURPOSE**

The purpose of this Ordinance is to establish minimum design requirements, standards, and specifications for improvements within subdivisions and land developments.

### **§1-2-2 DEFINITIONS**

Definitions pertaining to this Ordinance shall be as provided in Section 1-1 of this code.

### **§1-2-3 APPLICABILITY AND EXEMPTION**

The improvement requirements specified in this code section (1-2) shall apply to all single-family subdivisions and residential developments. The improvement requirements specified in this code section shall not apply to individual lots proposed for development as a detached, single-family dwelling or manufactured home, although such lots may be a part of a land subdivision that has initially met the requirements of this code section. All improvements required to be constructed as part of a subdivision shall be constructed and improved, in accordance with the standards and specifications for construction as required by this code section and as specified by the City.

No person to which this code section applies shall commence construction of any improvements on any land, prior to the approval of construction plans and engineering plans for said improvements, as required by Section 1-2 of this code, according to the improvement standards specified in this code section and as adopted by the City. No building permit shall be issued for a building, structure, or use, nor shall any excavation, grading, or land disturbance applications be approved, on any parcel of land that does not meet the improvement requirements specified in this section and as adopted by the City pursuant to this section.

#### §1-2-4 ENGINEERED DRAWINGS

Engineering drawings for public streets, including cross sections and centerline profiles, and public and private water, sewer, drainage, and utility systems, certified by a professional engineer registered in the State of Georgia, or if authorized under state law, a registered land surveyor, or professional landscape architect, shall be required to be submitted for review and approval, and such plans must meet the requirements of this code section (1-1) and the specifications of the City. Prior to approval and recording of a final plat a registered engineer for the subdivider/developer shall submit one copy of all finished, as-built plans of improvements, demonstrating that said improvements, as installed, meet the requirements of this code section and certifying that the plans accurately reflect actual construction and installation. The City shall maintain all as-built street and utility plans for future use by the City.

#### 1-2-5 PERMITS FOR CONSTRUCTION IN PUBLIC RIGHT-OF-WAY

Permits from the Zoning Administrator shall be required for construction in any public right-of-way. Permits will not be issued until such time that plans have been submitted and approved by the City. Permit fees shall be approved by Resolution of the Mayor and City Council.

#### 1-2-6 IMPROVEMENTS TO ABUTTING LAND

§1-2-6.1 Abutting Land Improvements. For subdivisions and land developments that abut and access an abutting public street, the subdivider or land developer shall install curb and gutter, sidewalk, other road improvements, and, if required, a deceleration lane, according to standards and specifications of the City, along all abutting public streets. When a subdivision or land development uses an unpaved public right-of-way for access, the subdivider or land developer shall improve that right-of-way to a pavement width consistent with City road design standards. Said improvements shall be from the subdivision or land development entrance to the paved public road which the Code Enforcement Officer determines will be the primary direction of travel for residents of the subdivision or occupants of the land development.

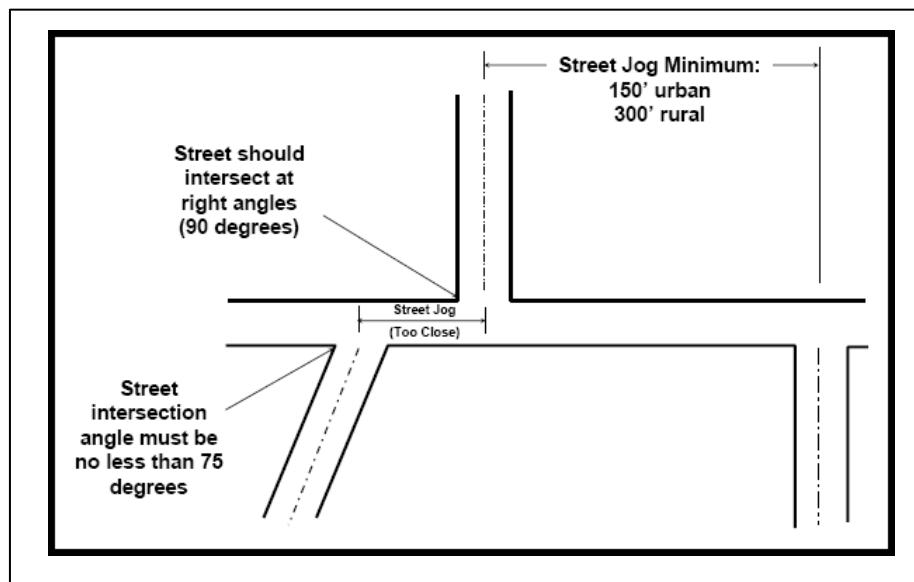
§1-2-6.2 Temporary Construction Exit. A temporary construction exit must be installed and maintained while construction and/or grading equipment will be leaving a construction site to a public right-of-way, street, or parking area until construction is completed.

- (a) The pad shall be adequate to substantially eliminate the transport of mud from the construction site by either motor vehicles and equipment or from water run-off.
- (b) Any materials or mud spilled, dropped, washed, or tracked from vehicles or from the site onto roadways or into storm drains must be removed immediately.
- (c) The exit must at least be 12 feet in width by 20 feet in length with a depth of 4 Inches.

§1-2-7 **STANDARDS FOR CONFIGURING NEW STREETS**

§1-2-7.1 **Street Alignment, Intersections and Jogs.** Streets shall be aligned to join with planned or existing streets. Under normal conditions, streets shall be laid out so as to intersect as nearly as possible at right angles (90 degrees), but in no case shall such a street intersection be less than 75 degrees. Where street offsets or jogs cannot be avoided, offset "T" intersections shall be separated by a minimum centerline offset of 150 feet in urban areas and 300 feet in rural areas. (See Figure 1-2-8.1.1).

**Figure 1-2-8.1.1**  
Intersection Angles and Street Jogs



§1-2-7.2 **Continuation of Existing Streets and Connections.** Existing streets, and their rights-of-way, shall be continued at the same or greater width, but in no case less than the required width. The Zoning Administrator and City Engineer may require that a subdivision provide one or more future connections to adjoining subdivisions or unsubdivided tracts.

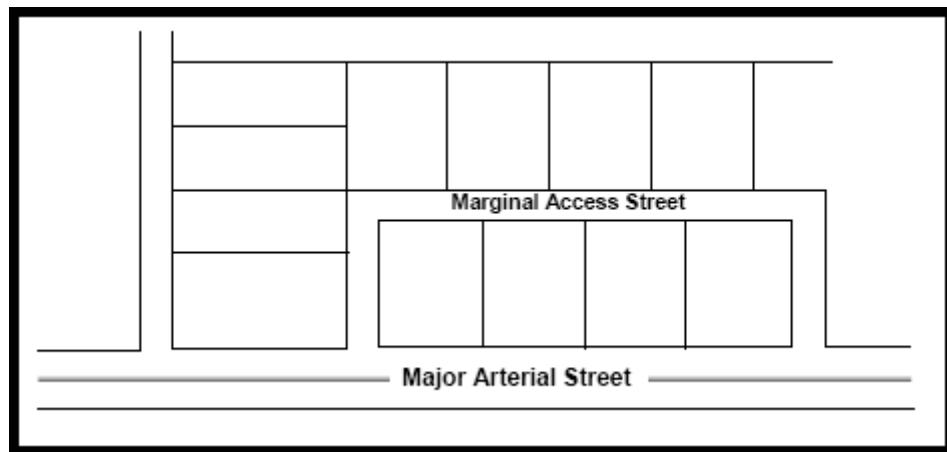
§1-2-7.3 **Street Plans for Future Phases of the Tract.** Where the plat or site plan proposed to be subdivided or developed includes only part of the tract owned or intended for subdivision or development by the subdivider or land developer, a tentative plan of a future street system for the portion not slated for immediate subdivision consideration may be required by the Code Enforcement Officer and if required shall be prepared and submitted by the subdivider or land developer.

§1-2-7.4 **Dead-End Streets and Cul-De-Sacs.** Streets that dead-end shall terminate in a cul-de-sac. The maximum length of such streets shall be 600 feet in urban areas and

1,200 feet in rural areas. Streets that are planned to continue at some future date shall provide a temporary cul-de-sac as required by the City.

§1-2-7.5 **Marginal Access Streets.** Whenever a subdivision is proposed abutting the right-of-way of a major arterial roadway, a marginal access street approximately parallel and adjacent to such right-of-way may be required by the Planning and Zoning Commission at a distance suitable for the appropriate use of land between such marginal access street and highway right-of-way. The Zoning Administrator and City Engineer may also require a 20-foot no-access easement and planting strip along the major arterial street to ensure that lots fronting on said street do not have access thereto. (See Figure 1-2-8.5.1).

**Figure 1-2-8.5.1**  
**Marginal Access Street**



§1-2-7.6 **Alleys and Service Access.** Alleys may be provided. If they are provided, they must be paved. Dead-end alleys shall be avoided were possible; but if unavoidable, they shall be provided with adequate turn-around facilities. Service access shall be provided to commercial and industrial developments for off-street loading, unloading, and parking consistent with and adequate for the uses proposed.

## §1-2-8 REQUIREMENTS FOR STREETS

§1-2-8.1 **Bridges.** Bridges on public rights-of-way shall meet current American Association of State Highway and Transportation Officials standards, as determined by the City Engineer.

§1-2-8.2 **Grading and Stabilization of Street Rights-Of-Ways.** When a new public street is proposed, all trees, brush, stumps, rocks, or other debris shall be cleared from the street right-of-way, except in cases where trees are required to be preserved by the Code Enforcement Officer in a manner acceptable to the City. All streets shall be graded to lines, grades and cross sections approved on plans. All unsurfaced

disturbed portions of street rights-of-way shall be stabilized by seeding, fertilizing, and mulching or by another equally effective method.

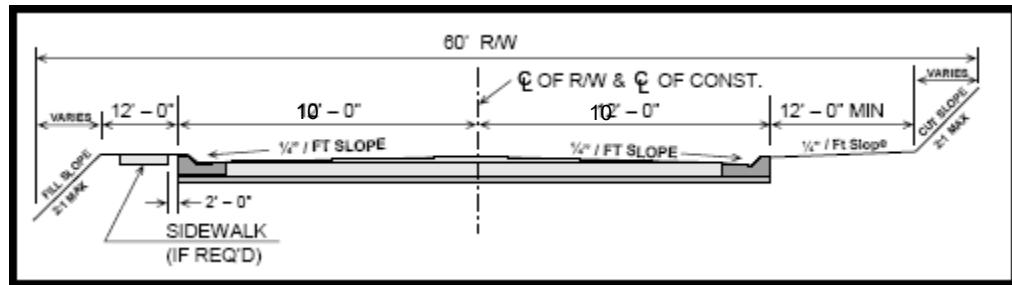
§1-2-8.3 Radius at Street Intersections. The right-of-way radius at street intersections shall be a minimum of 15 feet, with larger radii for streets serving nonresidential development, as approved by the City. The minimum pavement (curb) radius at street intersections shall be 25 feet with streets having an angle of intersection of 90 degrees. Where the angle of street intersection is less than 90 degrees, a longer radius may be required by the City.

§1-2-8.4 Street Grades. No street grade shall be less than one and one-half (1½) percent. No street grade for an arterial or collector street shall exceed eight percent. No other local street grade shall exceed 12 percent, unless the City finds that due to topographic conditions, a steeper grade is necessary, in which case the street grade shall not exceed 15 percent. Grades between 12 percent and 15 percent shall not exceed a length of 150 feet.

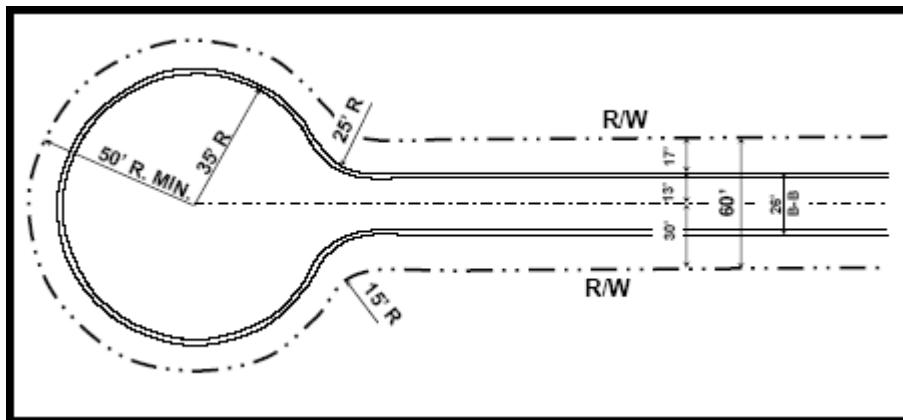
§1-2-8.5 Minimum Street Right-Of-Way and Pavement Widths. Street right-of-way and pavement widths shall at minimum meet the following:

STREET TYPE	MINIMUM RIGHT-OF-WAY WIDTH (FEET)	MINIMUM PAVEMENT WIDTH (FEET)
Major Arterial Street	100	24 (edge of pavement to edge of pavement)
Minor Arterial Street	80	24 (edge of pavement to edge of pavement)
Collector Street	60	24 (edge of pavement to edge of pavement)
Local street with curb and gutter	60	22 (back of curb to back of curb)
Local street without curb and gutter	60	20
Cul-de-sac turn around radius (non-commercial and non-industrial)	50	35 (back of curb)
Cul-de-sac turn around radius (commercial and industrial)	90	75 (back of curb)
Alley	25	18

**Figure 1-2-9.5.1**  
**Residential Street with Curb and Gutter**  
**(Cross Section Detail)**



**Figure 1-2-9.5.2**  
**Residential Cul-de Sac Detail**



§1-2-8.6 Street Horizontal Alignment and Reverse Curves. Street horizontal alignments and reverse curves shall at minimum meet the following:

STREET TYPE	MINIMUM HORIZONTAL RADII OF CENTER LINE CURVATURE (FEET)	MINIMUM TANGENTS BETWEEN REVERSE CURVES (FEET)
Major Arterial Street	1,250	250
Minor Arterial Street	1,000	200
Collector Street	500	100
Local Street with curb and gutter	100	100
Local Street without curb and gutter	100	100
Dead-end Street	100	100

§1-2-8.7 Base and Paving Non-Commercial or Non-Industrial. Base and paving in all noncommercial or non-industrial developments and manufactured housing parks shall at least meet the following minimum specifications:

- (a) The road or street must be graded and must achieve a minimum ninety-five (95) percent modified compaction using a "sheep foot roller" or the equivalent. The City must inspect the graded road bed before base material may be applied.
- (b) Following inspection of the road bed, a minimum of six (8) inches of approved Graded Aggregate Base (GAB) must be applied. Base material and installation must be inspected by the City prior to the application of any hard surface (paving). The paving contractor is required to have an inspection sticker issued by the Georgia Department of Transportation (GDOT) on the asphalt spreader and chip spreader.
- (c) The City reserves the right to require core testing to assure compliance.

Subject to provisions of §1-1-13.5 the developer shall have 180 days from approval, as provided in subsection (b), for paving to be completed. Roads or streets shall be paved using asphaltic concrete (plant mix). Specifications for plant mix asphalt shall be as follows: 1) any plant mix asphalt must comply with GDOT guidelines for the appropriate road or street; 2) plant mix asphalt shall, under all circumstances, include 2 inches of a binder layer with 1.5 inches of surface course

- (d) The developer shall furnish and maintain all necessary barricades and warning signs in the right-of-way while roadway frontage improvements are being made.
- (e) The City reserves the right to require geotechnical evaluation if the road fails a proof roll.
- (f)

§1-2-8.8 Base and Paving Commercial or Industrial. Base and paving in all non-commercial or non-industrial developments and manufactured housing parks shall at least meet the following minimum specifications:

- (a) The road or street must be graded and must achieve a minimum ninety-five (95) percent modified compaction using, a "sheep foot roller" or the equivalent. The City must inspect the graded road bed before base material may be applied.
- (b) Following inspection of the road bed, a minimum of eight (8) inches of approved aggregate base must be applied. The requirements shall be site-specific and follow Georgia Department of Transportation (GDOT) guidelines based on soil and anticipated traffic load and verified by the GDOT pavement design tool. Base material and installation must be inspected by the City prior to the application of any hard surface (paving). The paving contractor is required to have an inspection sticker issued by GDOT on the asphalt spreader and chip spreader.
- (c) The City reserves the right to require core testing to assure compliance.

Subject to provisions of §1-1-13.5 the developer shall have 180 days from approval,

as provided in sub-section (b), for paving to be completed. Roads or streets shall be paved using asphaltic concrete (plant mix). Specifications for plant mix asphalt shall be as follows: 1) any plant mix asphalt must comply with GDOT guidelines for the appropriate road or street; 2) all streets shall have a binder layer of minimum thickness of two (2) inches of 19mm superpave with a surface course of two inches of 2.5 mm superpave. All roads must be verified by the GDOT pavement design tool and soils be verified by geotechnical soil testing

- (d) The developer shall furnish and maintain all necessary barricades and warning signs in the right-of-way while roadway frontage improvements are being made.
- (e) The City reserves the right to require geotechnical evaluation if the road fails a proof roll.
- (f)

## 1-2-9 CURB CUTS AND ACCESS SPECIFICATIONS

§1-2-9.1 Entrance Improvement Specifications. Roadway entrances and improvements, including necessary acceleration and/or deceleration lane(s) and right/left turn lanes, shall be designed, installed, and maintained as approved by the State Department of Transportation, as applicable, or the City, in accordance with State or City specifications. All entrances or exits of any street or driveway, public or private, from or to any state highway shall be approved by the State Department of Transportation and the City prior to the construction of such entrances or exits and prior to the issuance of any building permit for any improvement to be served by such entrances or exits. All entrances or exits of any street or driveway, public or private, from or to any City street shall be approved by the City prior to the construction of such entrances or exits and prior to the issuance of any building permit for any improvement to be served by such entrances or exits.

1-2-9.2 Curb Cut Specifications. No curb cut or access driveway shall be permitted to be located closer than 100 feet to the nearest existing or proposed right-of-way of an intersecting roadway or closer than 40 feet to a side property line unless the adjacent property owner is in agreement with the encroachment of the driveway and approval is obtained from the City. Curb cuts or access driveways shall be no narrower than 24 feet from back of curb to back of curb. Strict adherence to these requirements may not be practical in all instances as determined by the City. The City may limit the maximum width of a curb cut and/or the number of curb cuts to a parcel as necessary when it is deemed to be of benefit to the safety and welfare of the public.

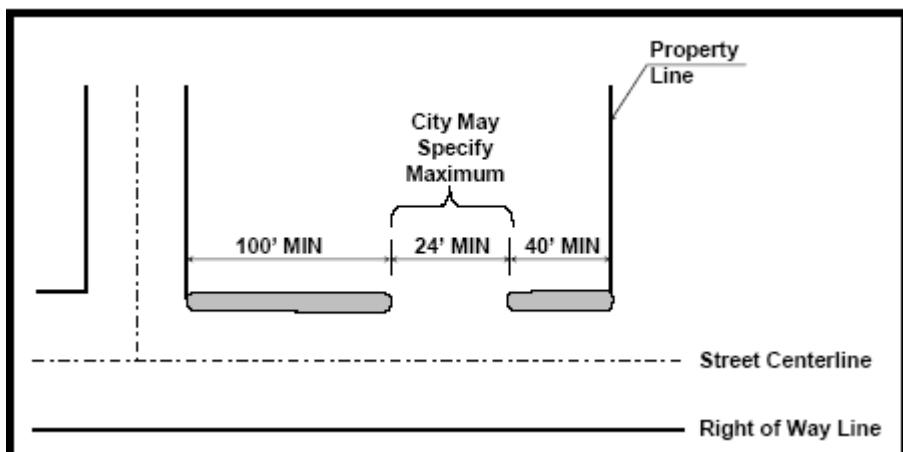
The following factors may be considered during the review and approval of a specific location of an entrance: the location of existing or planned median breaks; separation requirements between the entrance and major intersections; separation requirements between other entrances; the need to provide shared access with other sites; the need to align with previously approved or constructed access points on the opposite side of the street; sight distances; and the minimum number of entrances needed to move traffic onto and off the site safely and

efficiently.

1-2-9.3 Access Along and Near Divided Highways. Where a divided highway exists or is planned, the following access standards shall be met (see Figure 1-2-9.3.1):

Minimum Access Separation Requirements	Distance (Feet)
Curb cut of driveway from street intersection with divided highway	600
Parallel frontage road from right-of-way of divided highway	450
Curb cut or driveway on a local road from right-of-way of divided highway	300
Curb cut or driveway on a local road from state highway	200
Curb cut or driveway on parallel frontage road from local road	150

**Figure 1-2-9.3.1**  
Curb Cut and Access Specifications

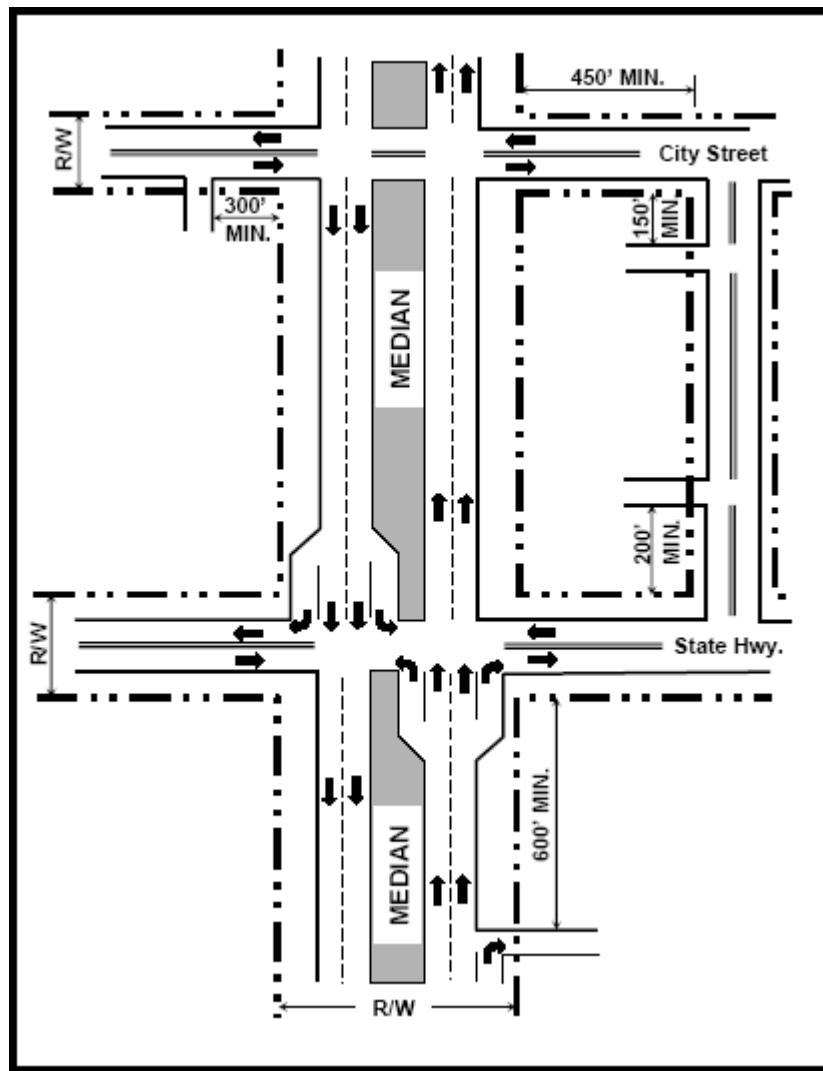


§1-2-9.4 Interparcel Connections. New development that contains or is intended to contain more than one building or use on site shall provide connections so that automobile trips between and among such buildings or uses can be accomplished without using the highway or major street. Where possible and practical, new developments and substantial improvements to existing developments shall provide for pedestrian and automobile access connections between adjacent properties under different ownership when the uses of the properties are of such compatibility that patrons may frequent both buildings or uses in the same vehicle trip. (See Figure 1-2-9.4.1).

§1-2-9.5 Access to Abutting Public Roads. Direct access from a subdivision to existing abutting collector streets is limited to one access for each 200 residential dwelling units. Individual lots of a subdivision may not directly access existing state, county, or city streets or roadways. If there are more than 200 residences and a second access to an existing collector road is not available or, in the opinion of the city engineer, could induce non-residential traffic through the development, a

single entrance may be allowed. However, such single entrance must be designed with the necessary traffic control and/or sufficient right-of-way and improvements including protected left-turn lanes to provide for the safe and efficient operation of the intersection, subject to the approval of the City.

**Figure 1-2-9.4.1**  
Minimum Access Spacing Along and Near Divided Highway



#### §1-2-10 STREET LIGHTING

The City may require that subdivisions and land developments in urban and suburban areas provide street lighting along all public streets and along existing streets abutting the subdivision or land development. Such street lighting if required shall meet specifications of the City.

#### §1-2-11 STREET SIGNS

Signs for street names, directions of travel, traffic control, and hazards shall be provided as directed by the City. Street signs on exterior/boundary streets shall be installed by the City with the developer paying a proportionate share determined by the City. Street signs for interior

streets of a subdivision or land development shall be installed at the subdivider or developer's expense by the subdivider or developer, subject to the approval of the City.

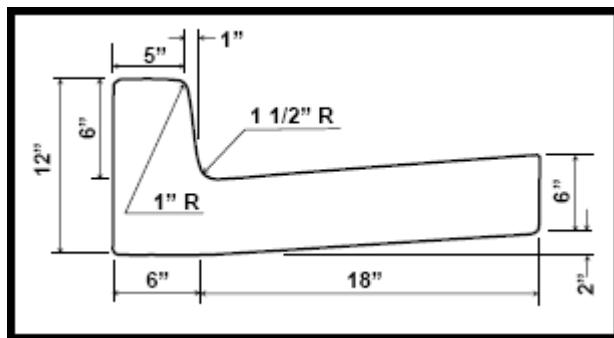
Unless otherwise provided in standards and specifications adopted by the City, street signs shall meet the following specifications. Signs shall be constructed of aluminum sheets with reflective backgrounds. Information on the street name signs shall be readable from both sides of the sign. Signs shall be installed on a steel post. The vertical distance from the road elevation to the bottom of the sign face shall be seven feet with a minimum burial depth of three feet.

#### §1-2-12 CURBS AND GUTTERS

Curbs and gutters shall be installed if required by the City in accordance with standards and specifications of the City. Subdivisions consisting totally of lots intended for single-family residential use containing a minimum of two acres shall not require curbs and gutters, provided, however, that curbs are required for all roads when sidewalks are required by these regulations. All commercial and industrial subdivisions and land developments must have curbs and gutters, regardless of the size of the lots. When property fronting on an existing City street is subdivided or developed, and the subdivision or land development uses said existing street for access, then curb and gutter shall be required along said street along the entire property frontage of said street.

Curbs shall be concrete which shall be class A 3000 psi strength at 28 days. The typical curb minimum section shall be six inches by 24 inches by 12 inches (Figure 1-2-12.1).

**Figure 1-2-12.1**  
Vertical Curb Detail



All streets and roads not required to include curbs and gutters shall be graded, paved, and drained to meet all construction and drainage standards for ditches, slopes, and grassing according to specifications established by the City Engineer.

#### 1-2-13 SIDEWALKS

§1-2-13.1 When Required. Sidewalks shall be provided on all major arterial, minor arterial, collector streets and local streets, unless the City determines that no public need exists for sidewalks in a certain location. Sidewalks shall be required when land

developments and subdivisions are located within one-mile of a public school or public parks. Sidewalks are required to be installed along both sides of the street internal to a subdivision, except in cases where the average lot size of the subdivision is two acres or more.

§1-2-13.2 Location. Sidewalks shall be included within the dedicated non-pavement right-of-way of roads and shall parallel the street pavement as much as possible; provided, however, the City may permit sidewalks to be designed and constructed so that they meander around permanent obstructions or deviate from a linear pattern for design purposes.

§1-2-13.3 Specifications. Sidewalks shall be a minimum of five feet wide. A median strip of grassed or landscaped areas at least two feet wide shall separate all sidewalks from adjacent curbs in residential areas.

#### §1-2-14 DRAINAGE AND STORMWATER MANAGEMENT

§1-2-14.1 General Requirements. An adequate drainage system, separate and independent of any sanitary sewer system and including any necessary ditches, pipes, culverts, intersectional drains, drop inlets, bridges, etc., shall be provided for the proper drainage of all surface water for all subdivisions and land developments. Sizing and location of all drainage structures shall be the responsibility of a registered professional engineer or land surveyor. The City may require the use of on-site control methods such as retention or detention to mitigate the stormwater and drainage impacts of the proposed subdivisions and land developments. The Zoning Administrator shall not approve any preliminary plat of subdivision that does not make adequate provision for storm and flood water runoff channels or basins as determined by the City Engineer. No building permit shall be issued for any building within a subdivision or for the development of land, if there is not present throughout the subdivision or to the land development an adequate system of drainage and stormwater management.

§1-2-14.2 Method of Design and Capacity. Storm sewers, where required, shall be designed by the SCS Method, and a copy of design computations shall be submitted along with required plans. Drainage improvements shall accommodate potential runoff from the entire upstream drainage area within the site and shall be designed to prevent increases in downstream flooding. Capacity for a 10-year storm or rain shall be provided for all street drainage structures such as catch basin, inlets cross drains, etc. Capacity for a 100-year frequency storm event shall be provided for all main drainage structures such as retention basins, principal storm sewers, and all types of flood protection works.

§1-2-14.3 Location. Drainage facilities shall be located in the road right-of-way where feasible, and shall be constructed in accordance with standards and specifications of the City. Catch basins shall be located at low points of streets. Where topography or other conditions are such as to make impractical the inclusion of

drainage facilities within road rights-of-way, perpetual, unobstructed easements at least twenty (20) feet in width for drainage facilities shall be provided across property outside the road right-of-way and with satisfactory access to the road. If sanitary sewer lines are excessively deep, wider easements shall be required to maintain a 1:1 open cut slope.

§1-2-14.4 Discharge. Drainage shall be designed so as to avoid concentration of storm drainage water from each lot or land development site to adjacent lots, land development sites, or vacant properties. Stormwater shall not be discharged directly to perennial streams. It shall be directed toward natural drainages. If water must be discharged to a stream, the water quality flowing into the stream must meet or exceed the water quality in the receiving waters. The water quantity flowing into the stream must be evaluated to ensure the stream channel can accommodate the increased flows and not disrupt or degrade the ecology of the water body. Refer to the Stormwater Post Construction Ordinance for further regulation.

§1-2-14.5 Grading and Site Drainage. Lots or land development sites shall be laid out so as to provide positive drainage away from all buildings, and drainage for individual lots or land development sites shall be coordinated with the general storm drainage pattern for the area. Buildings and parking lots shall be appropriately drained so as to prevent damage to abutting properties or public streets. All disturbed or graded ground areas of a building site not used for buildings or open storage areas shall be appropriately stabilized and grassed or covered with plants or landscaping materials.

§1-2-14.6 Cross Drainpipes. Where a watercourse separates the buildable area of a lot from the street by which it has access, provisions shall be made for installation of a culvert or other structure, the design of which shall be approved by the City Engineer. Cross-drains shall be provided to accommodate all-natural water flow, and shall be of sufficient length to permit full-width roadways and the required slopes. Cross drainpipes shall have head walls of an approved type on inlet and outlet ends of the pipe. Pipe installed within the right-of-way shall be reinforced concrete pipe. Other materials can be approved at the discretion of the Zoning Administrator. All storm drainpipes shall be minimum 18 inches in diameter. Storm sewer slopes shall be equal to or greater than one percent.

§1-2-14.7 Drop Inlets. Drop inlets shall be generally three-foot by three-foot boxes with two-foot by three-foot grates unless otherwise specified by the City Engineer.

§1-2-14.8 Easements. Where an irrigation ditch or channel, natural creek, stream or other drainage way crosses a subdivision or land development, the subdivider or developer shall provide an easement sufficient for drainage and maintenance. Easements shall be provided for all drainage facilities as approved by the City. When a subdivision or land development is traversed by a watercourse, drainage way, channel, or intermittent stream, a stormwater or drainage easement of at least twenty (20) feet shall be provided.

§1-2-15      WATER

§1-2-15.1 Generally. All habitable buildings and buildable lots shall be connected to a water system capable of providing water for health and emergency purposes, including adequate fire protection. No building permit shall be issued for any building within a subdivision or for the development of land, if there is not present throughout the subdivision or to the land development an adequate water supply.

§1-2-15.2 Water Main Requirements. When a public water main is accessible, the developer shall install adequate water facilities, including fire hydrants, according to specifications of the City. All water mains shall normally be at least six inches in diameter except that pipe of lesser size may be used if properly looped and adequate water pressure is maintained in accordance with standards established by the Southeastern Fire Underwriters Association. Pipe of less than four inches shall not be used except in unusual cases. Water lines shall be installed at least 30 inches below grade. Water mains within subdivisions and land developments must be provided with connections to each lot in the subdivision and each land development, except as otherwise specifically provided.

§1-2-15.3 Wells. If a county and/or municipal water supply is not available to the subdivision or land development at the time of constructing improvements for a subdivision or land development, then the subdivider or developer shall provide an adequate alternative water source and an adequate water storage facility. In subdivisions or land developments with a residential density of one unit per acre or less and when a public water system is not available as determined by the City, individual wells may be used in a manner so that an adequate supply of potable water will be available to every lot in the subdivision or to the land development. When individual wells are proposed to be used for water supply, water samples shall be submitted to the Bartow County Health Department for its approval, and individual wells shall be approved by the Bartow County Health Department. Approvals shall be submitted to the Code Enforcement Officer prior to final subdivision plat approval.

1-2-15.4 Community Water System. If a county and/or municipal water supply is not available to the subdivision or land development at the time of constructing improvements for a subdivision or land development, then the subdivider or developer shall provide an adequate alternative water source and an adequate water storage facility. Any community water system, if permitted, shall provide a minimum flow of 400 gallons per day per each lot platted, whether or not each lot is to be immediately developed; shall be sanitary; and shall have a minimum pressure of 30 pounds per square inch at each lot in the subdivision or each land development to be served. For all common non-public water supply systems, acceptable management, maintenance, and distribution policies and procedures shall be established. These policies and procedures shall be required to guarantee the provision of adequate supplies to each perspective lot owner on a continuing, ongoing basis, and to provide acceptable means for repairs and unforeseen events. The community water system plan shall be approved by the Bartow County Health Department and a letter of approval from the Georgia Department of Natural Resources shall

accompany the final plat or land development application.

1-2-15.5 Fire Hydrants. Fire hydrants shall be required for all nonresidential land developments and all subdivisions except those permitted to be served by individual on-site wells. Fire hydrants with appropriate water pressure at appropriate intervals throughout the subdivision or land development shall be provided by the subdivider or land developer as required by the Bartow County Fire Department. Fire hydrants shall be located no more than 1,000 feet apart and within 500 feet of any principal dwelling. Hydrants, fittings, valves and fire department connections shall be approved by the Fire Department. Fire department connections shall be not less than 18 inches or more than 36 inches above the level of the adjoining ground or paving. The thread of such connections shall be uniform with that used by the Fire Department. To eliminate future street openings, all underground utilities for fire hydrants, together with the fire hydrants themselves, and all other supply improvements shall be installed before any final paving of a street within the right-of-way shared by such underground utilities.

## §1-2-16 SEWER

§1-2-16.1 Generally. All habitable buildings and buildable lots shall be served by an approved means of wastewater collection and treatment. Each subdivision and land development shall be served by adequate sewage disposal facilities. No building permit shall be issued for any building within a subdivision or for the development of land, if there is not present throughout the subdivision or to the land development an adequate system of wastewater collection and treatment.

§1-2-16.2 Connection to Public Sewerage System. When a public sanitary sewerage system is reasonably accessible, as determined by the City, the subdivider or land developer shall connect with same and provide sewers accessible to each lot in the subdivision or to each land development. If a public sanitary sewer is reasonably accessible, it shall be unlawful to maintain upon any such property an individual sewage disposal system. When a public sanitary sewerage system is not immediately accessible but is anticipated by the City to be available within a period of three years, the applicant shall install sanitary sewer lines, laterals, and mains from the street curb to a point in the subdivision or land development boundary so that a future connection with the public sewer main can be made. The City may condition the approval of a subdivision or land development on the agreement to connect to the public sewerage system upon its availability. Sanitary sewers shall be located within street or alley rights-of-way unless topography dictates otherwise. No public sewer shall be less than eight inches in diameter. Manholes shall be installed in sanitary sewers with a maximum distance between two manholes of 400 feet, unless otherwise specified by standards of the City. Sanitary sewer slopes shall be equal to or greater than 0.7 percent for eight-inch lines. All sewer lines shall be designed with slopes to obtain a minimum velocity of two feet per second. Minimum 20-foot-wide easements shall be provided for all sanitary sewer lines.

§1-2-16.3 Alternative Provision. If sanitary sewer is not available at the time of the development of the subdivision or land development, and if sanitary sewer is not anticipated to be available within a period of three years to serve the subdivision or land development in question, then on-site septic tanks, an oxidation pond, or another approved method of treatment of sanitary sewerage shall be installed by and at the expense of the subdivider, land developer, or lot purchaser, in conformity with the requirements of the Bartow County Health Department and according to specifications adopted by the City.

1-2-16.4 Septic Tanks. Where individual onsite wastewater disposal systems are allowed and proposed, individual lot sizes and shapes must exhibit appropriate regard for the peculiar health, drainage, and maintenance characteristics on the site. Additionally, detailed soil tests may be required in order to verify the ability of the lots to safely contain and dispose of septic system effluent. All septic tanks and onsite wastewater disposal systems are subject to the approval of the Bartow County Health Department.

## §1-2-17 UTILITIES

§1-2-17.1 Placement of Utilities. All utility facilities, including but not limited to gas, electric power, telephone, and cable television, shall be located underground throughout the subdivision or land development. Whenever existing utility facilities are located above ground, except when existing on public roads and rights-of-way, they shall be removed and placed underground. Where possible, utilities shall be placed in the right-of-way. If right-of-way placement is not possible, easements centered on rear lot lines shall be provided for utilities, private and public, and such easements shall be at least 10 feet wide. When topographical or other conditions are such as to make impractical the inclusion of utilities along the rear lot lines of a subdivision, lot, or land development site, perpetual unobstructed easements at least 10 feet in width shall be provided alongside lot lines with satisfactory access.

§1-2-17.2 Utilities in Streets. All utilities to be installed in the streets shall be placed and compacted prior to paving. Where possible, the utilities must be placed in a sleeve installed under the road during road construction. After road construction is completed, there shall be no cuts to a finished road with approval from the Zoning Administrator or City Engineer.

## §1-2-18 OVERSIZING OF IMPROVEMENTS AND UTILITIES

The subdivider or land developer shall construct such oversized improvements and utilities that the City determines are necessary, provided that the subdivider or land developer shall not be obligated for the additional cost of improvements and utilities that are not uniquely required for that development, and provided the subdivider agrees to a proposal by the City to share in the cost arrangements for over-sizing improvements and utilities. A formula may be developed by

the City to provide for a sharing of the cost of other improvements needed to serve the subdivision or land development when certain of the improvements are necessary to serve future subdivisions or developments in the vicinity.

1-2-19     PROCEDURE FOR ADMINISTRATIVE INSPECTION AND ACCEPTANCE OF PUBLIC IMPROVEMENTS

Upon completion of public improvement construction, the subdivider or land developer shall notify the Code Enforcement Officer and request an inspection. The City Engineer shall inspect all public improvements and shall notify the subdivider or land developer by mail of non-acceptance or preliminary acceptance. If the public improvements are not acceptable, the reason for non-acceptance shall be stated and corrective measures shall be outlined in a letter of notification. Upon notification, the subdivider or land developer shall correct all deficiencies identified in the nonacceptance letter within the time limit established by the City Engineer. Once deficiencies are corrected, the subdivider or land developer shall again request inspection in writing. Acceptance of public improvements required by Section 1-2 of this code to be approved by the City shall be forwarded to the Mayor and City Council by the Code Enforcement Officer following receipt of written approval of the City Engineer.

## **§1-3 LEGAL STATUS PROVISIONS**

### **§1-3-1 SEVERABILITY**

It is intended the provisions of these regulations be severable and should any portions be held invalid, such invalidity shall not affect any other portions of these regulations.

### **§1-3-2 VIOLATIONS AND PENALTIES**

Any violation of any provisions of the subdivision regulations shall constitute a misdemeanor. In case of multiple violations, each violation shall be considered separately, and each day of violation shall constitute a separate offense, and shall be punished as provided by law.

### **§1-3-3 CONFLICTING ORDINANCES**

Whenever the provisions of these regulations and those of some other statute, ordinance or resolution apply to the same subject matter, the statute, ordinance or resolution requiring the highest, or more strict, standard shall apply.

### **§1-3-4 EFFECTIVE DATE**

These regulations shall take effect December 1, 1986, and be in force from and after its adoption by Euharlee, Georgia, the public welfare demanding.

ADOPTED SEPTEMBER 2, 2003.

AMENDED SEPTEMBER 6, 2005.

AMENDED SEPTEMBER 20, 2006.

AMENDED MARCH 7, 2023

AMENDED

**EUHARLEE, GEORGIA**

**, Mayor**

**ATTEST:**

**, City Manager**